



Transportation Technical Advisory Committee

(TTAC) Meeting Agenda

DATE: December 7, 2023

TIME: 10:00 a.m.

LOCATION: Payson Council Chambers | 303 North Beeline Hwy, Payson, AZ 85541

Access to the meeting room will be open to the public approximately 15 minutes before the meetings starts.

VIRTUAL OPTION: <https://us02web.zoom.us/j/82542379244?pwd=SGdqcnhkK2hxWnJkVVVRZTJTUUpNUT09>

ID NO: 825 4237 9244

PASSWORD: 156325

CALL-IN #: 1 (877) 853-5257 (If no mic on device)

I. Call to Order – Chair Rick Powers

II. Pledge of Allegiance

III. Roll Call

IV. Introductions & Title VI Notice

V. Approval of Minutes

A. August 10, 2023

P – F – T

B. October 12, 2023

P – F – T

VI. Call to the Public (Members of the public may speak on any item not listed on the agenda. Items presented during the Call to the Public portion of the Agenda cannot be acted on by the TTAC. Individual TTAC members may ask questions of the public but are prohibited by the Open Meeting Law from discussing or considering the item among themselves until the item is officially placed on the agenda. Individuals are limited to a two-minute presentation. For the sake of efficiency, the Chair may eliminate the Call to the Public portion of any agenda.)

VII. Standing Reports

A. Member Jurisdictions

All

Info.

B. Multi-Modal Planning Division, ADOT

Will Randolph

Info.

C. Local Public Agency, ADOT

LPA Staff

Info.

D. District, Engineers, ADOT

District Engineers

Info.

E. CAG Transportation Planning Update

Travis Ashbaugh

Info.

VIII. New Business

A. CAG STBG/HURF Exchange Applications

Travis Ashbaugh

P – F – T

B. Transportation Improvement Program (TIP)

Travis Ashbaugh

P – F – T

C. Round Table

All

Info.

D. Future Agenda Items

All

Discussion

IX. Scheduling of Next Meetings – Tentatively

Thursday, January 11, 2024 – 10:00 AM

ZOOM Webinar

(Webinar info will be posted approximately one week prior to meeting date)

Thursday, February 8, 2024 – 10:00 AM

ZOOM Webinar

(Webinar info will be posted approximately one week prior to meeting date)

X. Adjournment

Approved by
(Andrea Robles, CAG Executive Director)

DATE: August 10, 2023

TIME: 10:00 a.m.

LOCATION: CAG Conference Room | 2540 West Apache Trail, Suite 108, Apache Junction, AZ 85120
With ZOOM Webinar Option

MEMBERS PRESENT:

Rick Powers - *Chair*
(Globe)

Thomas Goodman
(Gila County)

Gloria Ruiz
(Winkelman)

Larry Halberstadt – *Vice Chair*
(Payson)

Steve Abraham
(Pinal County)

Tim Grier
(Star Valley)

Lana Clark
(Superior)

Alexis Rivera
(Miami)

Will Randolph
(ADOT - MPD)

MEMBERS ABSENT:

VACANT
(Hayden)

LaReesa Sanchez
(White Mountain Apache Tribe)

VACANT
(Mammoth)

Tyler Bingham
(Kearny)

Sandra Shade
(Ak-Chin Indian Community)

Barney Bigman
(San Carlos Apache Tribe)

GUESTS PRESENT:

Luis Chavez
(Globe)

Mark Henige
(ADOT LPA)

Tina Woody
(Star Valley)

Dale Miller
(Rick Engineering)

Homer Vela
(Gila County)

Christine Smith
(Payson)

Bobby Davis
(Star Valley)

Sylvia Kerlock
(Winkelman)

CAG Staff:

Andrea Robles
(Executive Director)

Travis Ashbaugh
(Transportation Planning Director)

I. Call to Order

Chair Powers called the meeting to order at 10:00 AM.

II. Pledge of Allegiance

Chair Powers led the Committee in the Pledge of Allegiance.

III. Roll Call

Roll call was taken. Nine (9) voting members were present, constituting a quorum as established by the CAG TTAC Bylaws.

IV. Introductions & Title VI Notice

Introductions were made on the Webinar. Mr. Ashbaugh read a statement of where and how to file a complaint regarding Title VI violations.

V. Call to the Public

No one answered the Call to the Public.

VI. Consent Agenda

Mr. Halberstadt made a motion to approve the consent agenda as presented. Mr. Goodman seconded the motion. The motion passed unanimously.

A. Approval of Minutes – (July 13, 2023)

The July 13, 2023 CAG Transportation Technical Advisory Committee meeting minutes were approved under Consent Item VI-A.

B. CAG/SCMPO JPA for Mobility Management Services

The CAG/SCMPO Joint Project Agreement for Mobility Management Services was approved under Consent Item IV-B.

VII. New Business

A. FY25 RTAC Priority Project Recommendations

Mr. Ashbaugh stated that all applicants will have five minutes to present their projects. He provided a spreadsheet that provided the order of priority based on the rankings. He also stated that that scores were reviewed in three comparable scenarios:

1. Raw scores provided by the those who ranked the projects.
2. Scores that took out the highest and lowest scores from each project.
3. Scores that took out the highest and lowest scores from each category within each project.

Mr. Ashbaugh stated that none of the projects shifted within the ranks of priority when comparing the three scoring scenarios. After the Committee members reviewed the priority ranking sheet for accuracy of information, each applicant presented their projects to the committee for consideration.

Mr. Ashbaugh stated the total amount being requested by member entities is slightly over the threshold amount in which CAG is allotted to plan for within the FY25 RTAC Priority Project initiative. He stated that the total amount submitted is \$267,507.79 over the planning threshold given by the RTAC Board. Mr. Ashbaugh provided three options in which to address the overage:

- Option 1: Reduce the full \$267,507.79 amount from the Town of Miami's request due to their application being ranked last.
- Option 2: Drop Pinal County's "Dillstone/Carlsbad/Baywood/Cedar Crest Improvements" seeking \$238,579.00 of state appropriations and reduce the Town of Miami's request by \$28,928.79.
- Option 3: Drop the Town of Miami's request all together as the last ranking application and spread the available amount of \$2,057,642.21 among the other projects.

Mr. Ashbaugh stated "Option 3" is not ideal and would like to see everyone being good partners in this planning process and see that the Town of Miami be funded. He also asked the Committee for other options other than what was presented.

Ms. Clark also stated she would like to see Miami funded and suggested that Pinal County drop their “Calle Futura Street & Neal Street Improvements” project and spread the available funds (\$816,942.21) among other projects. Many of the Committee members concurred they would like to see funding for the Town of Miami’s request.

Mr. Abraham stated that Pinal County is willing to accept “Option 2” and drop their lowest requested amount application. Mr. Ashbaugh stated that with this option, the Committee would still need to cut out \$28,928.79. Ms. Clark suggested that it should be cut from one of Pinal County’s other projects since they had multiple project requests. Ms. Woody suggested reducing the remaining amount equally among the other projects. Mr. Miller suggested reducing the amount proportionately based on the requested amounts. He stated this would allow projects with lower requested amounts to absorb less of a hit in the funding, which are typically from the smaller communities.

Mr. Goodman made a motion to accept all projects as presented by priority, drop Pinal County’s “Dillstone/Carlsbad/Baywood/Cedar Crest Improvements” application for the requested amount of \$238,579.00 and reduce the remaining \$28,928.79 amount equally among the remaining projects. Mr. Halberstadt seconded the motion. A roll call vote was taken. There were seven (7) ayes, one (1) nay (Star Valley), and one (1) abstention (ADOT). The motion passed. The priority of the projects and their State Budget requested amounts are as follows:

Project Name	State Budget Request
1. Globe – Yuma Street Bridge	\$2,989,777.16
2. Winkelman/Hayden – Quarelli Street & Golf Course Road – Phase 2	\$2,166,375.69
3. Payson – McLane Road Improvements	\$3,406,785.69
4. Star Valley – Local Street Improvements	\$2,301,411.69
5. Superior – Lobb/Neary/Kellner Improvements	\$1,528,467.69
6. Gila County – Young Road (NFSR) 512	\$1,796,785.69
7. Pinal County #1 – Calle Futura Street & Neal Street Improvements	\$1,081,235.69
8. Pinal County #2 – McNab Parkway Improvements	\$2,439,155.69
9. Miami – Bridge & Local Street Improvements	\$2,321,935.69
Total	\$20,031,930.68

B. Transportation Improvement Program

Mr. Ashbaugh stated that the “Golden Hill – New Sidewalk” project (Project #s GIL 18-01D & GIL 22-01C), and the “Main Street – New Pedestrian Sidewalk” project (Project # GIL 09-01T) are now complete and that Gila County is requesting to remove them from the TIP.

Mr. Grier made a motion to remove “Golden Hill – New Sidewalk” project (Project #s GIL 18-01D & GIL 22-01C), and “Main Street – New Pedestrian Sidewalk” project (Project # GIL 09-01T) from the FY24-FY29 CAG TIP. Mr. Abraham seconded the motion. The motion passed unanimously.

VIII. Standing Reports

A. Member Jurisdictions

No updates were provided.

B. Multi-Modal Planning Division, ADOT

Mr. Randolph reported on ten (10) items:

1. **FY25-FY29 Five-Year Construction Program:** Project nominations from ADOT technical groups were in June 2023. The call for district rankings was recently released with the deadline at the end of July 2023, and the district workshops are scheduled for the end of August through September 2023. The P2P process will prioritize all highway construction projects for consideration in ADOT's FY25-FY29 Five Year Construction Program.
2. **Federal Discretionary Grants:** Please continue to visit USDOT's website or the ADOT grant coordination page <https://azdot.gov/planning/adot-grant-coordination-group> for NOFOs and deadlines.
3. **ADOT SMART Program:** Please continue to work with your local agencies to submit any applications for the program. Information on the program's website can be found at <https://azdot.gov/planning/grant-coordination/az-smart-fund>.
4. **TA Program:** The call for projects was released the week of July 20, 2023. The program website is live at <https://activetransportation.a.gov/transportation-alternatives-program>. The website has a recorded webinar, guidance, FAQs, links to the application and more. The Phase 1 screening application must be submitted before the August 31, 2023 deadline. Project sponsors whose application meets the Phase 1 screening criteria will be invited by email to apply to the Phase 2 evaluation process. Project sponsors will need to submit their Phase 2 evaluation applications before the September 29, 2023 deadline.
5. **Long Range Transportation Plan:** The LRTP is still in the first public review period for the DRAFT LRTP for a 45-day comment period. Comments will be addressed in the plan with a revised draft that will go to the Governor's Office, ADOT Executive Leadership and the State Transportation Board in September 2023. Then a final 30-day public comment period will occur before it goes to the State Transportation Board for final approval in October 2023. Please visit the ADOT LRTP webpage <https://www.adot2050plan.com> to review the plan documentation, summary sheets and provide comments.
6. **Statewide Rest Area Study:** The study has now been completed and the final report can be downloaded from the project website.
7. **Electric Vehicle (EV) Infrastructure Plan:** ADOT is conducting its annual update of the NEVI plan as required from the USDOT/Joint Energy Office. A Statewide Virtual Public Meeting for the EV Plan Update took place on July 18, 2023. A request for information was sent to EV Industries on June 29, 2023 to gain feedback and perspectives for the future procurement of vendors regarding the EV installation. ADOT will be looking at conducting RFPs in the later part of this year or into 2024. For more information visit ADOT's EV plan website and sign up to the mailing list to receive study updates by email and opportunities to provide input. <https://azdot.gov/planning/transportation-studies/arizona-electric-vehicle-program>
8. **Statewide Truck Parking Plan:** The project is on schedule. The consultant is identifying locations of undesignated truck parking along the state highway facilities. They will further analyze the undesignated parking results to determine any insight on the causes and truck parking deficits. The consultant is also working on benefit/cost analysis, alternative materials, and locations for potential truck parking expansions/conversions at existing ADOT facilities. The next TAC/Stakeholder Meetings will occur on August 18, 2023.

9. **Wireless Communications Facilities Program Guidelines Study:** The draft Program Guideline comments have been received from the TAC and select industry. Comment resolution should be completed along with the Final Program Guidelines. The project is scheduled to be completed by the end of August 2023.
10. **Statewide ITS Architecture Update:** The data collection process is nearing its end with the participating stakeholders. If any local agencies can send in their inventory and need surveys that would greatly be appreciated. The prior 208 project lists have also been sent to local agencies for comments and updates for the current update.

C. Local Public Agency, ADOT

No updates were provided.

D. District, Engineers, ADOT

No updates were provided.

E. CAG Transportation Planning Update

Mr. Ashbaugh reported on three (3) items:

1. CAG staff attended a roundtable event with Senator Kyrsten Sinema regarding transportation in Pinal County. CAG staff has been in discussion with Senator Sinema's staff to extend an invite to a future Regional Council meeting to provide the opportunity to voice any transportation related issues and/or concerns, help in seeking congressional appropriations. Senator Sinema had stated during the roundtable she is looking for ideas on how to improve the federal discretionary grant processes, etc.
2. Registration for the Arizona Rural Transportation Summit is open and will be held in Yuma on October 18-19, 2023. Suggestions to begin securing hotel rooms were given as they go quickly.
3. The last remaining traffic counts to be conducted will occur later in August 2023 and includes count locations that will need to be recounted due to data reading errors or wrongful placement of counter.

F. Round Table

No updates were provided.

G. Future Agenda Items

Mr. Miller suggested having a "Funding Source 101" class to discuss the many different grants available throughout the year. Chair Powers stated that we had done that in the past and may be good to do it again. Mr. Miller also suggested looking into creating a local depository of recent bids for just the CAG Region in order to understand local cost estimates for projects.

IX. Scheduling of Next Meetings

Mr. Ashbaugh stated that next TTAC meeting is scheduled for September 14, 2023 and is planned to be in Payson. Should the September 2023 meeting be canceled, the October 12, 2023 meeting will be planned for Payson. Either meeting will be at Council Chambers located at 303 North Beeline Highway, Payson, AZ 85541, at 10:00 AM. A virtual option will also be available.

X. Adjournment

Chair Powers adjourned the meeting at 11:39 AM.

DATE: October 12, 2023
TIME: 10:00 a.m.
LOCATION: via ZOOM Webinar

MEMBERS PRESENT:

Rick Powers - *Chair*
(Globe)

Thomas Goodman
(Gila County)

Gloria Ruiz
(Winkelman)

Larry Halberstadt – *Vice Chair*
(Payson)

Steve Abraham
(Pinal County)

Barney Bigman
(San Carlos Apache Tribe)

Lana Clark
(Superior)

Alexis Rivera
(Miami)

Will Randolph
(ADOT - MPD)

MEMBERS ABSENT:

VACANT
(Hayden)

LaReesa Sanchez
(White Mountain Apache Tribe)

VACANT
(Mammoth)

Tyler Bingham
(Kearny)

Sandra Shade
(Ak-Chin Indian Community)

Tim Grier
(Star Valley)

GUESTS PRESENT:

Luis Chavez
(Globe)

Mark Henige
(ADOT LPA)

Dale Miller
(Rick Engineering)

Sharay Satchell
(ADOT MPD)

Homer Vela
(Gila County)

Christine Smith
(Payson)

CAG Staff:

Travis Ashbaugh
(Transportation Planning Director)

I. Call to Order

Chair Powers called the meeting to order at 10:00 AM.

II. Pledge of Allegiance

Chair Powers led the Committee in the Pledge of Allegiance.

III. Roll Call

Roll call was taken. Nine (9) voting members were present, constituting a quorum as established by the CAG TTAC Bylaws.

IV. Introductions & Title VI Notice

Introductions were made on the Webinar. Mr. Ashbaugh read a statement of where and how to file a complaint regarding Title VI violations.

V. Approval of Minutes – (August 10, 2023)

Committee members stated that the PDF file of the Draft meeting minutes for the August 10, 2023 meeting was not opening up correctly and was suggested to be brought back for approval at the next meeting. No action was taken.

VI. Call to the Public

No one answered the Call to the Public.

VII. Standing Reports

A. Member Jurisdictions

Payson

Mr. Halberstadt reported on three (3) item:

1. No bids were received for construction on the **“Granite Dells Road”** project (**Project # PAY 21-01C**). ADOT informed the Town that since the project is a “HURF Exchange” project, alternative delivery methods can be used. The Town is negotiating with a contractor for pricing and a schedule for the construction work needed. Once the negotiations are completed, the Town will more than likely request a third extension to complete the project.
2. The Design for the **“Intersection: W. Longhorn & S. McLane Road – (Roundabout)”** project (**Project # PAY 19-02D**) is approximately 95 percent complete. Once the last five (5) percent has been completed, the Right-of-Way acquisition will begin. The construction has been funded via Legislative action in the FY24 State Budget cycle.
3. A “Transportation Alternatives” application was submitted for Houston Mesa Road to add bike lanes and sidewalks where gaps currently exist.

Gila County

Mr. Goodman reported on two (2) items:

1. Design on the **“Houston Mesa Road”** HSIP project (**Project # GIL 23-02D**) is approximately 60 percent complete. The County was informed that the project is short of \$1.3 million in funding due to the rise in construction. The County is requesting the funding needed from the Gila County Board of Supervisors to keep the project moving forward. The County also found out that a portion of the project falls within the Town of Payson and therefore an agreement between the County and the Town will need to be created to keep the section within the project. Kimley Horne is the consultant working on the project.
2. Design on the **“Control Road – Segment 1”** HSIP project (**Project # GIL 23-03D**) is approximately 60 percent complete. The County was informed that the project is short \$600,000 in funding, also due to the rise in construction. The County is requesting the funding needed from the Gila County Board of Supervisors to keep the project moving forward as well. Kimley Horne is the consultant working on the project.

San Carlos Apache Tribe

Mr. Bigman reported on three (3) items:

1. The Tribe is looking to pave and chip seal many of the subdivision roads and have sent out a Request for Quotes. To date, three to four vendors have responded and hope to approve a vendor soon.
2. The Tribe is working on a Right-of-Way survey update through Rick Engineering. The survey is needed in order to be eligible for federal funding for future construction projects.

3. The **“White Mountain Road (BIA 10) & Airport Road”** street light project (**Project # SCA 21-01D**) ran into Design issues. The original plan was to use the existing utilities poles within the San Carlos Irrigation District easement. The District stated they wouldn’t allow that, constituting the need to redesign.

Globe

Mr. Chavez reported on two (2) items:

1. A scoping document is being prepared for the **“Globe/Gila County Sidewalk Improvements”** project (**Project # GLB 23-01C**).
2. Bid openings for the **“Pinal Creek Bridge – Cottonwood St (Structure #9711)”** construction project (**Project # GLB 22-01C**) is scheduled to start October 20, 2023. ADOT will be administering the project. Construction is expected to begin in February 2024.

Pinal County

Mr. Abraham reported on one (1) item:

1. The Road Safety Assessments (RSA) in the Saddlebrook and Oracle regions of the county are moving forward. The ADOT Vulnerable Road User Safety Assessment Draft was recently released and one of the locations identified that has the second highest bicycle fatality rate was within the area. Once completed, Pinal County will send their RSA data to ADOT, in the hope of seeking funding to address the safety needs.

B. Multi-Modal Planning Division, ADOT

Mr. Randolph reported on eight (8) items:

1. **FY25-FY29 Five-Year Construction Program:** District workshops were complete in the month of September 2023. The ADOT P2P Manager is working on incorporating workshop comments and project updates into the P2P project list. The P2P process will prioritize all highway construction projects for consideration in ADOT’s FY 2025-2029 Five Year Construction Program.
2. **ADOT SMART Program:** Please continue to work with your local agencies to submit any applications for the program. Information on the program’s website can be found at <https://azdot.gov/planning/grant-coordination/az-smart-fund>.
3. **Federal Discretionary Grants:** Please continue to visit USDOT’s website or the ADOT grant coordination page <https://azdot.gov/planning/adot-grant-coordination-group> for NOFOs and deadlines.
4. **TA Program:** Project applications were accepted up to August 31, 2023 for phase one of the process. Project Sponsors whose application met the phase one screening criteria were invited to apply to the phase two evaluation process with a submission deadline of September 29, 2023. From October 4-13, 2023, the TA Program Technical Advisory Committee (TAC) reviewed and scored the applications.
5. **Long Range Transportation Plan:** The first 45-day public review of the DRAFT LRTP was completed on September 7, 2023. ADOT received over 485 comments from 120 respondents. There were 67 attendees that submitted 73 comments on the Statewide Virtual Meeting held on August 22, 2023 alone. The public comments were reviewed and sent to the Governor’s Office, ADOT, Executive Leadership, and the State Transportation Board in mid-September 2023. Final approval will be requested at the October 20, 2023 Arizona State Transportation Board meeting.
6. **Electric Vehicle (EV) Infrastructure Plan:** The annual update of the NEVI Plan received approval on September 30, 2023 as required per the Joint Office of Energy and Transportation. ADOT will be looking at conducting RFPs later this year or into 2024.

7. **Statewide Truck Parking Plan:** The last stakeholder meeting occurred on October 3, 2023 to discuss the undesignated parking analysis, implementation strategies, possible alternative materials for paving, and locations for potential truck parking expansions/conversions at existing ADOT facilities. The draft final report is expected to be completed by October 27, 2023.
8. **Statewide ITS Architecture Update:** The data collection is mostly completed. The prior 2018 project lists have been sent to local agencies for any updates to be implemented in the project list, with a few follow-ups to local agencies. ADOT provided its own project updates to the consultant from the last 2018 effort with new ones to be added in from the new ITS master plan.

C. Local Public Agency, ADOT

Mr. Henige reported on two (2) items:

1. The Federal Highway Administration (FHWA) is hammering down on the naming convention for federalized projects. All future project names must comply with FHWA's guidelines in naming a project. Possible training may be coming in the near future on how to properly name a project.
2. The call for FY25 Off-System Bridge/Bridge Formula funding will be coming out before the end of the month. The due date for applications is December 31, 2023.

D. District, Engineers, ADOT

No updates were provided.

E. CAG Transportation Planning Update

Mr. Ashbaugh reported on four (4) items:

1. The final traffic count data has been received and will be entered for counts conducted in 2023.
2. Registration for the Arizona Rural Transportation Summit is open and will be held in Yuma on October 18-19, 2023.
3. The Selection Committee for the Pinal County Coordinated Mobility Gap Analysis selected AECOM as the consultant. The next steps is to prepare and approve the contract.
4. The next step for the Gila County Intergovernmental Public Transportation Authority (GCIPTA) is to schedule the first Board meeting. Finding a viable meeting date has been the delay at this point. Many transitional documents will also need to be created in order for the existing transit programs to be transitioned to the GCIPTA.

VIII. Old Business

A. Transportation Improvement Program (TIP)

Mr. Ashbaugh informed the Committee that the Design for the **"BIA 170 (New Sidewalk)"** project (**Project # SCA 28-01D**) that was recommended to be advanced to FY24 by the Committee, was tabled at the last Regional Council meeting. He stated the Council made the decision to table the amendment, which was based on a technical matter involving the dues from San Carlos Apache Tribe to be a member of CAG. He stated once CAG receives the membership dues, the amendment would go back to the Regional Council for consideration.

Mr. Ashbaugh also stated that Mr. Bigman had the desire to advance the construction phase of the **"BIA 170 (New Sidewalk)"** project (**Project # SCA 30-01C**) but will be addressed in a future meeting.

Mr. Ashbaugh asked the local agencies who had submitted projects to request Congressional Appropriations if they have heard any updates. He stated the projects are listed within the TIP with a non-funded status. None of the local agencies have heard any updates since their submittals.

IX. New Business

A. Call-for-Projects (CAG STBG TIP Funds)

Mr. Ashbaugh stated since one of the major projects originally on the TIP has now been funded with State Appropriations, the funding within the TIP would need to be reprogrammed. He stated there is approximately \$1.1 million available collectively within the current six-year TIP. He emphasized that although the Committee had recommended moving to a five-year TIP, he wanted to provide all members an opportunity to apply for additional funding for current projects, due to inflation, or program new projects since there hasn't been a Call-for-Projects for some time. Mr. Ashbaugh proceeded to run through the application schedule with a due date of November 16, 2023.

Mr. Ashbaugh also stated that when the current application was revised, there was a strong consensus that once funds were available again, scoping projects would be a focus as many of the smaller local agencies do not have an engineer. He stated this would be the opportunity to request funding for such items.

F. Round Table

Chair Powers stated that the bid opening for the ADOT "Queen Creek Bridge" in Superior is scheduled for October 13, 2023. He stated that updates would be appreciated from the corresponding ADOT District Office in future meetings.

G. Future Agenda Items

Chair Powers stated at previous meeting, there were discussions on preparing a standardized unit cost template that could be used to help with construction estimates. Mr. Henige stated that the E2C2 program has a unit cost breakdown of recent winning bids.

Mr. Ashbaugh stated that Dale Miller from Rick Engineering mentioned during the previous meeting to build a regional unit cost template. Discussions pursued back and forth on if it would be useful considering how larger the CAG Region is. Mr. Ashbaugh requested a refresher training course on the E2C2 program at a future meeting from Mr. Henige.

X. Scheduling of Next Meetings

Mr. Ashbaugh presented the 2024 Calendar Year schedule for TTAC meetings. He restated the desire of the Committee to meet at least twice a year. He stated we can decide from one meeting to another which meeting dates they could be. He stated that the next scheduled meeting is on December 7, 2023 in Payson, AZ.

XI. Adjournment

Chair Powers adjourned the meeting at 11:40 AM.

CAG APPLICATION



SURFACE TRANSPORTATION BLOCK GRANT PROGRAM (STBGP) APPLICATION

GENERAL PROJECT INFORMATION					
SPONSORING AGENCY:	Central Arizona Governments	DATE SUBMITTED:	11/6/23		
CONTACT NAME:	Andrea Robles	TITLE:	Executive Director		
EMAIL ADDRESS:	Arobles@cagaz.org	PHONE #:	520-827-0707		
<input type="checkbox"/> ROADWAY IMPROVEMENT	Roadway Name:				
	Starting Location:				
	Ending Location:				
	Length (to the 0.1 of a mile):				
	# of Lanes (Before & After):	Before:		After:	
<input type="checkbox"/> INTERSECTION IMPROVEMENT	Roadway Name "A":				
	Roadway Name "B":				
<input type="checkbox"/> BRIDGE IMPROVEMENT	<input type="checkbox"/> Restoration/Operational	Bridge Sufficiency Rating (LINK to ADOT NBI Table)			
	<input type="checkbox"/> Replacement	Structurally Deficient?	<input type="checkbox"/> Yes	<input type="checkbox"/> No	
	<input type="checkbox"/> Widening	Functionally Obsolete?	<input type="checkbox"/> Yes	<input type="checkbox"/> No	
<input checked="" type="checkbox"/> OTHER	Description of project type:	FY 2024 Technical Assistance Funds			
FEDERAL FUNCTIONAL CLASSIFICATION (LINK: FEDERAL FUNCTIONAL CLASSIFICATION MAPS):		N/A			
AVERAGE ANNUAL DAILY TRAFFIC (AADT) COUNT: (LINK: AADT COUNTS):	N/A	DATE OF AADT COUNT:	N/A		

COST ESTIMATE & PROJECT PROGRAMMING

<input checked="" type="checkbox"/> DESIGN PLANNING	FY Program Year:	FY 2024	
	Funding Source Request:	<input checked="" type="checkbox"/> STBGP	<input type="checkbox"/> HURF Exchange
	Other Non-Local Funding Sources to be Utilized:	<input type="checkbox"/> _____	
	Total Cost Estimate:	\$26,511.13	
	Federal Share (STBGP or HURF Exchange):	\$25,000.00	
	Minimum Required Local Match (STBGP = 5.7%):	\$1,511.13 (In-Kind Match)	
	NOTE: HURF Exchange provides 90% of costs up front. The remaining 10% will be reimbursed upon project completion.		
<input type="checkbox"/> CONSTRUCTION	FY Program Year:		
	Funding Source Request:	<input type="checkbox"/> STBGP	<input type="checkbox"/> HURF Exchange
	Other Non-Local Funding Sources to be Utilized:	<input type="checkbox"/> _____	
	Total Cost Estimate:		
	Federal Share (STBGP or HURF Exchange):		
	Minimum Required Local Match (STBGP = 5.7%):		
	NOTE: HURF Exchange provides 90% of costs up front. The remaining 10% will be reimbursed upon project completion.		
<ul style="list-style-type: none"> If Utilizing STBGP dollars, ATTACH a completed “ADOT Cost Estimate Tool” document for your estimate. If Utilizing HURF Exchange dollars, ATTACH a completed “HURF Exchange Scoping, Cost and Schedule” document for your estimate. In the event that the proposed project within this application is considered ineligible or is not awarded with HURF Exchange dollars and would still like to be considered for STBGP funds, please fill out the “ADOT Cost Estimate Tool” document in addition to the “HURF Exchange Scoping, Cost and Schedule” document. <p style="text-align: center; color: red; font-weight: bold; margin-top: 10px;">Any application without the required attachment(s) will not be considered for funding.</p>			

PROJECT NEED

This section should clearly state why this project is one of the highest priorities within the CAG Region for which the use of the requested regional funds is the best option *(No more than one page long; Cambria size 10 font)*.

PROJECT NEED:

CAG is requesting \$25,000 to assist with additional expenses in the Transportation Department and to supplement the CAG/ADOT Annual Work Program. In June 2021, CAG requested and received \$30,497 from the TIP. These funds enabled CAG to continue providing the services required by ADOT, since CAG has not received any increases to the Statewide Planning and Research (SPR) funds in over 20 years, even though administrative costs continue to rise. We were able to stretch those dollars until now.

Over the last two years CAG has been a leader with the efforts of the Greater Arizona Rural Transportation Advocacy Council (RTAC) Priority Projects. CAG was successful last year in advocating for projects within the Region. Agencies from CAG's transportation planning area received approximately \$11 million for transportation projects. Mr. Ashbaugh has facilitated an additional TIP process for member agencies to apply for projects. This includes review of applications, scoring and meeting presentations to the CAG TTAC, Management Committee, Regional Council and one-on-one meetings with your local legislators. Fact sheets are also created and become part of a booklet/package for all of Greater Arizona. Mr. Ashbaugh produces this booklet on behalf of Greater Arizona. CAG has recently provided a table to showcase the selected projects at the Rural Transportation Summits for the past three years. This has required additional resources, efforts and expenses to CAG's Transportation Department as well as the additional costs to provide handouts and the creation of board/displays for the projects.

We thank you in advance for your consideration and approval of this request,

PROJECT WORK DESCRIPTION

Provide a brief work description that describes the work to be performed, existing and/or proposed conditions, its benefits and overall cost estimate. *(No more than one page long; Cambria size 10 minimum font)*. **Please ATTACH a Project Vicinity/Project Location Map on a separate page as part of the overall application.**

PROJECT NEED:

To supplement the CAG/ADOT Annual Work Program.

ITEMS TO BE ADDRESSED

PROJECT INCLUSION IN PREVIOUS PLANS	Is the project included in previous plans?		<input type="checkbox"/> YES	<input type="checkbox"/> NO
	<input type="checkbox"/>	Regional Transportation Plan (RTP)	<input type="checkbox"/>	Pre-Scoping Studies
	<input type="checkbox"/>	Road Safety Assessment (RSA)	<input type="checkbox"/>	Comprehensive Economic Development Strategy (CEDS)
	<input type="checkbox"/>	Capital Improvement Program (CIP)	<input type="checkbox"/>	Local Comprehensive Plan / General Plan
	<input type="checkbox"/>	Local Transportation Plan	<input type="checkbox"/>	Other #1 _____
	<input type="checkbox"/>	Other #2 _____	<input type="checkbox"/>	Other #3 _____
COMMUNITY TRANSPORTATION BENEFITS	Does the project provide multi-modal improvements? Yes or No and Why?		N/A	
	Does the project provide Community Investments and/or Economic Development benefits? Yes or No and Why?			
SAFETY COUNTERMEASURES <i>(For Potential Use of HSIP Funds)</i>	Can you provide crash data, including fatalities over the last five (5) years? Yes or No? <i>(Cite Source of Crash Data)</i>		N/A	
	Does the project primarily include any of the 44 safety countermeasures listed on the next page? FHWA safety countermeasures Yes or No?			

SAFETY COUNTERMEASURE		Y or N
1. "Stop Ahead" pavement markings		N/A
2. "Vehicles Entering When Flashing" (VEWF) system (advance post mounted signs on major and loops on minor)		N/A
3. 12-inch signal heads all faces all directions		N/A
4. Actuated advance warning dilemma zone protection system		N/A
5. 3-inch yellow retroreflective sheeting to signal backplates		N/A
6. Advance street name signs		N/A
7. All red clearance interval new or existing signals		N/A
8. All-way stop control (with flashing beacons)		N/A
9. All-way stop control (without flashing beacons)		N/A
10. Composite shoulders (5 feet minimum) on rural two lane roads		N/A
11. 3-lane roadways with center turn lane		N/A
12. Flashing lights and sound signals at Railroad grade crossings		N/A
13. Gates with signs at railroad at grade crossings		N/A
14. Improve 2-lane roadway to 4-lane divided roadway		N/A
15. Improvements that include reducing 11 feet lanes to 9 feet		N/A
16. Install shoulder rumble strips		N/A
17. Install centerline rumble strips		N/A
18. Install wide edgelines (6-inch min)		N/A
19. Install a traffic signal (engineering study demonstrates meeting MUTCD Warrant 7)		N/A
20. Install dynamic signal warning flashers		N/A
21. Install dynamic speed feedback sign at high speed crash curve site with identified speeding problems		N/A
22. Install Intersection Conflict Warning Systems (ICWS) for 4-lane at 2-lane intersections		N/A
23. Install ICWS for 2-lane at 2-lane intersections		N/A
24. Install ICWS with a combination of overhead and advanced post mounted signs (various messages) and flashers		N/A
25. Install ICWS with overhead signs (various messages) and flashers at the intersection on minor; loop on major		N/A
26. Install ICWS with post mounted signs (various messages) and flashers in advance of the intersection on major; loop on major		N/A
27. Modern roundabout where a signalized intersection exists		N/A
28. Roundabout at a high-speed 3 or 4 leg rural intersection		N/A
29. Modify zero or negative left-turn lane offset to create positive offset		N/A
30. New left-turn lanes with positive offset		N/A
31. Pavement friction (Microsurfacing, Open Graded Friction Course, High Friction Surfacing)		N/A
32. Pedestrian Hybrid Beacon (PHB or HAWK)		N/A
33. Position offset left-turn lanes on both major road approaches		N/A
34. Protected only left-turn signal equipment		N/A
35. Protected-permissive left-turn signal equipment		N/A
36. Raised median		N/A
37. Right-turn lane geometry with increased line of sight		N/A
38. Rural 2-lane roads with TWLTL (Two-Way Left Turn Lanes)		N/A
39. Urban 2-lane road with TWLTL		N/A
40. Safety edge treatment on rural highways		N/A
41. Single- or multi-lane roundabout at a 2-way stop-controlled intersection		N/A
42. Single- or multi-lane roundabout at existing signalized intersection		N/A
43. 2-way stop control at uncontrolled neighborhood intersections		N/A
44. Wet-reflective pavement markings		N/A

OTHER CONSIDERATIONS

(Provide Any Supplemental Supporting Documentation – Optional)

ENVIRONMENTAL	<p>Are there any potential environmental impacts or challenges of the project that you can foresee?</p> <p>Yes or No and Why?</p> <p><i>(e.g. endanger species, cultural assets, hazardous materials sites, 4Fs, Title VI populations, wet lands that would be affected, etc.)</i></p>	N/A
RIGHT-OF-WAY (ROW)	<p>Please describe any ROW items associated with this project.</p> <p><i>(e.g. Will ROW be required? How much ROW? Is the State Land Department involved?)</i></p>	N/A
DEVELOPMENT ACTIVITY	<p>Is there any planned or ongoing development activity that could impact the proposed project? If Yes, please explain.</p>	N/A
UTILITIES	<p>Will the project include/require any utility relocation(s) by the project sponsor? If Yes, please explain.</p>	N/A
DRAINAGE	<p>Are there any drainage issues and/or proposed improvements associated with this project?</p>	N/A

LEVEL OF SERVICE (LOS):	Current:		After:	
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Level of Service “A” =	Free-flow traffic with individual users virtually unaffected by the presence of others in the traffic stream.
Level of Service “B” =	Stables traffic flow with a high degree of freedom to select speed and operating conditions but with some influence from users.
Level of Service “C” =	Restricted flow that remains stable but with significant interactions with others in the traffic stream. The general level of comfort and convenience declines noticeably at this level.
Level of Service “D” =	High-density flow in which speed and freedom to maneuver are severely restricted and comfort and convenience have declined even though flow remains stable.
Level of Service “E” =	Unstable flow at or near capacity levels with poor levels of comfort and convenience.
Level of Service “F” =	Forced traffic flow in which the amount of traffic approaching a point exceeds the amount that can be served. LOS F is characterized by stop-and-go waves, poor travel times, low comfort and convenience, and increased accident exposure.

GILA COUNTY APPLICATION



SURFACE TRANSPORTATION BLOCK GRANT PROGRAM (STBGP) APPLICATION

GENERAL PROJECT INFORMATION					
SPONSORING AGENCY:	Gila County Public Works	DATE SUBMITTED:	10/27/2023		
CONTACT NAME:	Thomas D. Goodman	TITLE:	County Engineer		
EMAIL ADDRESS:	tgoodman@gilacountyaz.gov	PHONE #:	(928) 402-8507		
<input checked="" type="checkbox"/> ROADWAY IMPROVEMENT	Roadway Name:	Young Road/National Forest System Road 512			
	Starting Location:	Southern Tonto National Forest Boundary			
	Ending Location:	Northern Tonto National Forest Boundary			
	Length (to the 0.1 of a mile):	13.5 miles			
	# of Lanes (Before & After):	Before:	2	After:	2
<input type="checkbox"/> INTERSECTION IMPROVEMENT	Roadway Name "A":				
	Roadway Name "B":				
<input type="checkbox"/> BRIDGE IMPROVEMENT	<input type="checkbox"/> Restoration/Operational	Bridge Sufficiency Rating (LINK to ADOT NBI Table)			
	<input type="checkbox"/> Replacement	Structurally Deficient?		<input type="checkbox"/> Yes	<input type="checkbox"/> No
	<input type="checkbox"/> Widening	Functionally Obsolete?		<input type="checkbox"/> Yes	<input type="checkbox"/> No
<input type="checkbox"/> OTHER	Description of project type:				
FEDERAL FUNCTIONAL CLASSIFICATION (LINK: FEDERAL FUNCTIONAL CLASSIFICATION MAPS):		Rural Minor Collector			
AVERAGE ANNUAL DAILY TRAFFIC (AADT) COUNT: (LINK: AADT COUNTS):		219	DATE OF AADT COUNT:		Year 2022

COST ESTIMATE & PROJECT PROGRAMMING

<input type="checkbox"/> DESIGN	FY Program Year:		
	Funding Source Request:	<input type="checkbox"/> STBGP	<input type="checkbox"/> HURF Exchange
	Other Non-Local Funding Sources to be Utilized:	<input type="checkbox"/> _____	
	Total Cost Estimate:		
	Federal Share (STBGP or HURF Exchange):		
	Minimum Required Local Match (STBGP = 5.7%):		
<i>NOTE: HURF Exchange provides 90% of costs up front. The remaining 10% will be reimbursed upon project completion.</i>			
<input checked="" type="checkbox"/> CONSTRUCTION	FY Program Year:	FY25	
	Funding Source Request:	<input checked="" type="checkbox"/> STBGP	<input type="checkbox"/> HURF Exchange
	Other Non-Local Funding Sources to be Utilized:	<input type="checkbox"/> _____	
	Total Cost Estimate:	\$1,210,000.00	
	Federal Share (STBGP or HURF Exchange):	\$1,100,000.00	
	Minimum Required Local Match (STBGP = 5.7%):	\$110,000.00	
<i>NOTE: HURF Exchange provides 90% of costs up front. The remaining 10% will be reimbursed upon project completion.</i>			
<ul style="list-style-type: none"> If Utilizing STBGP dollars, ATTACH a completed <u>"ADOT Cost Estimate Tool"</u> document for your estimate. If Utilizing HURF Exchange dollars, ATTACH a completed <u>"HURF Exchange Scoping, Cost and Schedule"</u> document for your estimate. In the event that the proposed project within this application is considered ineligible or is not awarded with HURF Exchange dollars and would still like to be considered for STBGP funds, please fill out the "ADOT Cost Estimate Tool" document in addition to the "HURF Exchange Scoping, Cost and Schedule" document. <p style="text-align: center; color: red; margin-top: 10px;">Any application without the required attachment(s) will not be considered for funding.</p>			

PROJECT NEED

This section should clearly state why this project is one of the highest priorities within the CAG Region for which the use of the requested regional funds is the best option *(No more than one page long; Cambria size 10 font)*.

PROJECT NEED:

Gila County has made the design and paving of Young Road/National Forest System Road (NFSR) 512 a top priority. Young Road is currently a two-lane, native surface, rural minor collector road. This project would reduce dust, improve visibility, and enhance safety for motorists and pedestrians.

Young Road/NFSR 512 is the main road for ingress and egress for the community of Young, Arizona and the primary access for recreational activities including camping, sightseeing, off highway vehicles, hunting, and fishing. Additionally, it provides primary access for economic activities including cattle grazing, mineral extraction, timber activities, tourism, agriculture, and wine production within and adjacent to the Pleasant Valley Ranger District.

The project would also improve response times for wildland firefighters and the Tonto's ability to protect valuable resources. The project is within the Colcord Fireshed that is one of the top 10 priority firesheds in the Southwestern Region. Improving conditions of the road would enhance the Tonto National Forest's ability to treat hazardous fuels in the area.

In addition to regional and local use, Young Road/NFSR 512 is utilized by many visitors from the greater Phoenix metropolitan area to escape the heat during the summer months, hunt in the fall, or enjoy snow related recreation in the winter.

Additionally, this project would reduce impacts on the road surface and adjacent land from erosion, in turn reducing maintenance costs. Paving Young Road/NFSR 512 could serve as a catalyst for significant economic development through increased use of renewable forest resources and improved access to Young and other small rural communities.

The project is fully supported by the Tonto National Forest and Forest Supervisor Neil Bosworth. Gila County currently has a Project Agreement with the Tonto National Forest to update Environmental Assessment. This update is underway and will be completed in September 2024.

In conclusion, this project will provide a safer and more reliable access to the community of Young and surrounding federal lands. Gila County considers this a top priority.

PROJECT WORK DESCRIPTION

Provide a brief work description that describes the work to be performed, existing and/or proposed conditions, its benefits and overall cost estimate. *(No more than one page long; Cambria size 10 minimum font)*. **Please ATTACH a Project Vicinity/Project Location Map on a separate page as part of the overall application.**

PROJECT NEED:

DESIGN – Design is expected to be completed by FY25 and will include 100% plans, specifications, and cost estimate from the 60% design plans scheduled for completion in September 2024. The 60% plans are part of an Environmental Analysis update funded by the Tonto National Forest in the amount of \$472,000.00. The environmental documents and clearances include an updated Environmental Assessment in support of paving and realignment efforts of Young Road/NFSR 512.

CONSTRUCTION – Approximately \$1,210,000.00 will be used for construction of selected segments. The road is currently a two-lane roadway with a native surface. This project will use either MAG or ADOT uniform standard specifications and details for paving:

- Survey
- Clearing and Grubbing
- Subgrade Preparation
- Aggregate Base Course Placement
- Final Grading
- Placement and Construction of Asphalt Concrete Pavement

The segments selected for paving will be determined from an engineering analysis and built as funding becomes available.

Gila County will match 10% of \$1,100,000.00

GILA COUNTY MATCH = \$110,000.00

TOTAL = \$1,210,000.00

ITEMS TO BE ADDRESSED

PROJECT INCLUSION IN PREVIOUS PLANS	Is the project included in previous plans?		<input checked="" type="checkbox"/> YES	<input type="checkbox"/> NO
	<input type="checkbox"/>	Regional Transportation Plan (RTP)	<input type="checkbox"/>	Pre-Scoping Studies
	<input type="checkbox"/>	Road Safety Assessment (RSA)	<input type="checkbox"/>	Comprehensive Economic Development Strategy (CEDS)
	<input checked="" type="checkbox"/>	Capital Improvement Program (CIP)	<input type="checkbox"/>	Local Comprehensive Plan / General Plan
	<input type="checkbox"/>	Local Transportation Plan	<input type="checkbox"/>	Other #1 _____
	<input type="checkbox"/>	Other #2 _____	<input type="checkbox"/>	Other #3 _____
COMMUNITY TRANSPORTATION BENEFITS	Does the project provide multi-modal improvements? Yes or No and Why?		No, this project is not focused on congestion reduction. Yes, Young Road/NFSR 512 is the main road for ingress and egress for the community of Young, Arizona and the primary access for recreational activities including camping, sightseeing, off highway vehicles, hunting, and fishing. Additionally, it provides primary access for economic activities including cattle grazing, mineral extraction, timber activities, tourism, agriculture, and wine production within and adjacent to the Pleasant Valley Ranger District.	
	Does the project provide Community Investments and/or Economic Development benefits? Yes or No and Why?			
SAFETY COUNTERMEASURES <i>(For Potential Use of HSIP Funds)</i>	Can you provide crash data, including fatalities over the last five (5) years? Yes or No? <i>(Cite Source of Crash Data)</i>		Yes, Gila County Sheriff can provide crash data. Crash data has been ordered from the Sheriff. Yes, safety edges may include guardrail, reflective edge lines, rumble strips, or other measures.	
	Does the project primarily include any of the 44 safety countermeasures listed on the next page? FHWA safety countermeasures Yes or No?			

SAFETY COUNTERMEASURE		Y or N
1. "Stop Ahead" pavement markings		N
2. "Vehicles Entering When Flashing" (VEWF) system (advance post mounted signs on major and loops on minor)		N
3. 12-inch signal heads all faces all directions		N
4. Actuated advance warning dilemma zone protection system		N
5. 3-inch yellow retroreflective sheeting to signal backplates		N
6. Advance street name signs		N
7. All red clearance interval new or existing signals		N
8. All-way stop control (with flashing beacons)		N
9. All-way stop control (without flashing beacons)		N
10. Composite shoulders (5 feet minimum) on rural two lane roads		N
11. 3-lane roadways with center turn lane		N
12. Flashing lights and sound signals at Railroad grade crossings		N
13. Gates with signs at railroad at grade crossings		N
14. Improve 2-lane roadway to 4-lane divided roadway		N
15. Improvements that include reducing 11 feet lanes to 9 feet		N
16. Install shoulder rumble strips		N
17. Install centerline rumble strips		N
18. Install wide edgelines (6-inch min)		N
19. Install a traffic signal (engineering study demonstrates meeting MUTCD Warrant 7)		N
20. Install dynamic signal warning flashers		N
21. Install dynamic speed feedback sign at high speed crash curve site with identified speeding problems		N
22. Install Intersection Conflict Warning Systems (ICWS) for 4-lane at 2-lane intersections		N
23. Install ICWS for 2-lane at 2-lane intersections		N
24. Install ICWS with a combination of overhead and advanced post mounted signs (various messages) and flashers		N
25. Install ICWS with overhead signs (various messages) and flashers at the intersection on minor; loop on major		N
26. Install ICWS with post mounted signs (various messages) and flashers in advance of the intersection on major; loop on major		N
27. Modern roundabout where a signalized intersection exists		N
28. Roundabout at a high-speed 3 or 4 leg rural intersection		N
29. Modify zero or negative left-turn lane offset to create positive offset		N
30. New left-turn lanes with positive offset		N
31. Pavement friction (Microsurfacing, Open Graded Friction Course, High Friction Surfacing)		N
32. Pedestrian Hybrid Beacon (PHB or HAWK)		N
33. Position offset left-turn lanes on both major road approaches		N
34. Protected only left-turn signal equipment		N
35. Protected-permissive left-turn signal equipment		N
36. Raised median		N
37. Right-turn lane geometry with increased line of sight		N
38. Rural 2-lane roads with TWLTL (Two-Way Left Turn Lanes)		N
39. Urban 2-lane road with TWLTL		N
40. Safety edge treatment on rural highways		Y
41. Single- or multi-lane roundabout at a 2-way stop-controlled intersection		N
42. Single- or multi-lane roundabout at existing signalized intersection		N
43. 2-way stop control at uncontrolled neighborhood intersections		N
44. Wet-reflective pavement markings		Y

OTHER CONSIDERATIONS				
(Provide Any Supplemental Supporting Documentation – Optional)				
ENVIRONMENTAL	<p>Are there any potential environmental impacts or challenges of the project that you can foresee?</p> <p>Yes or No and Why?</p> <p><i>(e.g. endanger species, cultural assets, hazardous materials sites, 4Fs, Title VI populations, wet lands that would be affected, etc.)</i></p>	No. An updated Environmental Analysis is scheduled for completion in September 2024 for the purpose of paving this road.		
RIGHT-OF-WAY (ROW)	<p>Please describe any ROW items associated with this project.</p> <p><i>(e.g. Will ROW be required? How much ROW? Is the State Land Department involved?)</i></p>	No		
DEVELOPMENT ACTIVITY	<p>Is there any planned or ongoing development activity that could impact the proposed project? If Yes, please explain.</p>	No		
UTILITIES	<p>Will the project include/require any utility relocation(s) by the project sponsor? If Yes, please explain.</p>	No		
DRAINAGE	<p>Are there any drainage issues and/or proposed improvements associated with this project?</p>	No		
LEVEL OF SERVICE (LOS):		Current:	A	After: A
Level of Service “A” =		Free-flow traffic with individual users virtually unaffected by the presence of others in the traffic stream.		
Level of Service “B” =		Stables traffic flow with a high degree of freedom to select speed and operating conditions but with some influence from users.		
Level of Service “C” =		Restricted flow that remains stable but with significant interactions with others in the traffic stream. The general level of comfort and convenience declines noticeably at this level.		
Level of Service “D” =		High-density flow in which speed and freedom to maneuver are severely restricted and comfort and convenience have declined even though flow remains stable.		
Level of Service “E” =		Unstable flow at or near capacity levels with poor levels of comfort and convenience.		
Level of Service “F” =		Forced traffic flow in which the amount of traffic approaching a point exceeds the amount that can be served. LOS F is characterized by stop-and-go waves, poor travel times, low comfort and convenience, and increased accident exposure.		

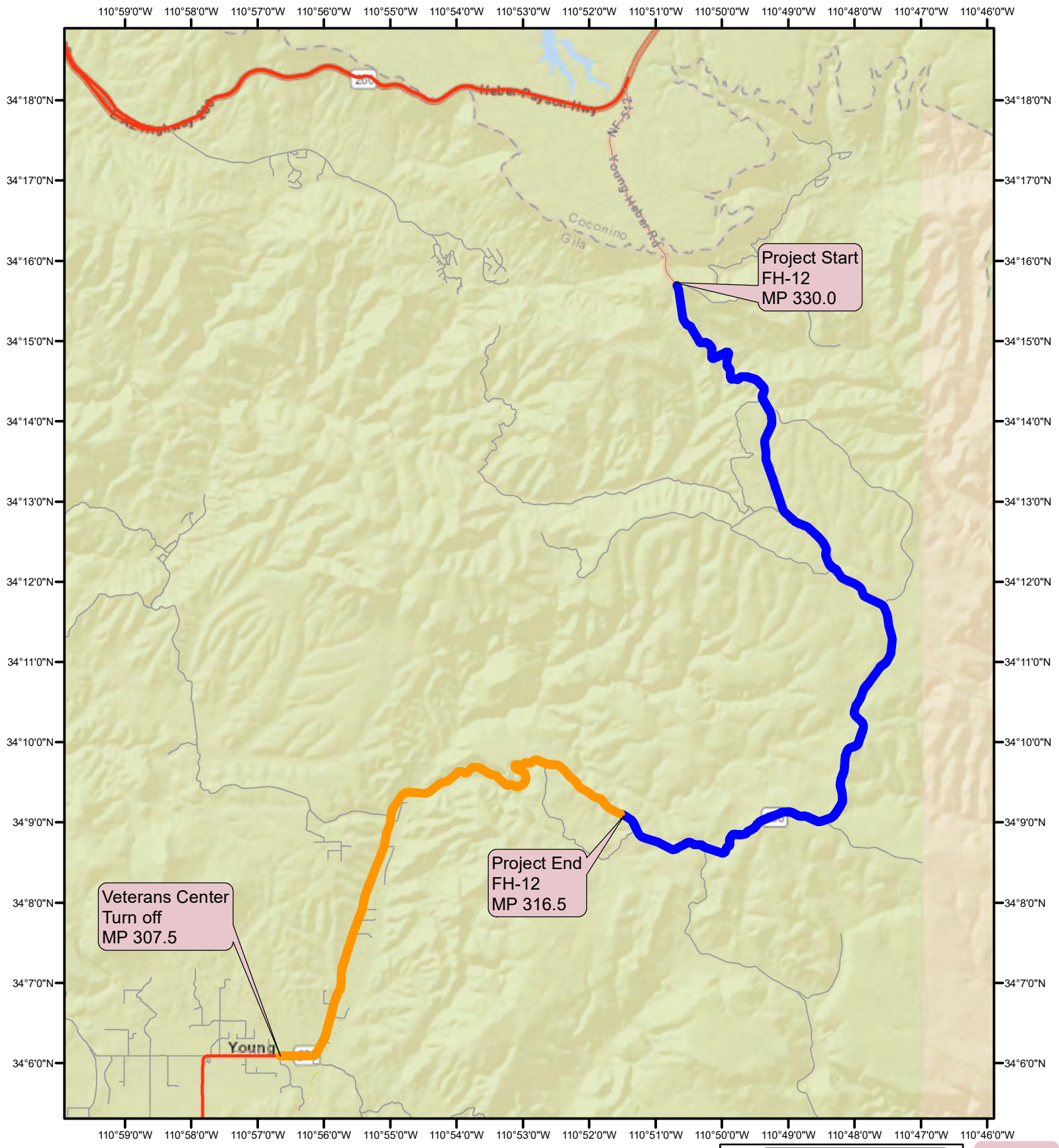
Estimated Project Costs						
INSTRUCTIONS: List all items necessary to develop and construct your project. The applicant is responsible for verifying all costs and their accuracy. Construction cost overruns will be the responsibility of the sponsoring agency.						
Enter values into GREEN CELLS.		The program will automatically calculate the Totals and Federal Share at 94.3%				
LOCAL PROJECTS: Please note that the Stage I Costs shown below are to be funded by the sponsoring agency and are not eligible for Federal Reimbursement.						
ITEM DESCRIPTION	UNIT	QUAN.	UNIT PRICE	TOTAL	FEDERAL FUNDS @ 94.3%	SPONSOR MATCHING FUNDS @ 5.7%
STAGE 1 – SCOPING (15% Preliminary Design)						
SCOPING COSTS						
Costs cannot be applied toward the federal participation or local match						
SITE TOPOGRAPHIC SURVEY (2%-5% of constr. cost) <i>(Enter \$0 in Unit Price column if none required)</i>	LS	1		\$0.00		
SCOPING DOCUMENT (Scoping Letter, Project Assessment or DCR)	LS	1		\$0.00		
ENVIRONMENTAL DETERMINATION (Including technical supporting documents)	LS	1	\$472,000.00	\$472,000.00		
HAZARDOUS MATERIALS ASSESSMENT Including heavy metals & asbestos (If an assessment is necessary, anticipate \$1,500. <i>Enter \$0 in Unit Price column if none required</i>)	LS	1		\$0.00		
SUBTOTAL – PROJECT SCOPING COSTS				\$ 472,000	\$445,096	\$26,904
STAGES II, III, IV - DESIGN (30%, 60%, 95%-100% Design)						
DESIGN COSTS						
Note: The use of federal funds for design is optional and subject to authorization. Design should not go beyond Stage II (30%) without environmental approval.						
PS&E's - Plans, Special Provisions, Cost Estimates & Schedules (10%-20% of construction cost.) (Shall be refunded if project is not constructed)	LS	1	\$0.00	\$0.00		
GEOTECHNICAL INVESTIGATION (If a report is necessary, anticipate 5% of construction cost) Includes testing, Geotech Report, Materials & Pavement Design Report) <i>Enter \$0 in Unit Price column if none required.</i>	LS	1	\$0.00	\$0.00		
DRAINAGE REPORT (If a report is necessary, anticipate 5% of construction cost) <i>Enter \$0 in Unit Price column if none required</i>	LS	1		\$0.00		
STORM WATER POLLUTION PREVENTION PLAN (Required if there is over 1 acre of total disturbance, 1% of construction cost) <i>Enter \$0 in Unit Price column if none required.</i>	LS	1		\$0.00		
SUBTOTAL – PROJECT DESIGN COSTS				\$ -	\$0	\$0
Federal Funds for design are calculated at 94.3% of the total design cost. If requesting less than 94.3% Federal Funds for design, enter new total or 0 in the Federal column.						

ITEM DESCRIPTION	UNIT	QUAN.	UNIT PRICE	TOTAL	FEDERAL FUNDS @ 94.3%	SPONSOR MATCHING FUNDS @ 5.7%
STAGE V – CONSTRUCTION						
SITE ACQUISITION & HARDSCAPE CONSTRUCTION						
RIGHT-OF-WAY ACQUISITION (if necessary)	LS	1		\$0.00	\$0.00	\$0.00
INSTALLATION OF STORMWATER POLLUTION PREVENTION MEASURES (If over 1 acre of disturbance, 5% of constr. costs) <i>Enter \$0 in Unit Price column if area of disturbance is less than one acre.</i>	LS	1	\$0.00	\$30,000.00	\$28,290.00	\$1,710.00
SITE PREPARATION (Clearing and grubbing, plant salvage)	LS	1	\$100,000.00	\$30,000.00	\$28,290.00	\$1,710.00
DEMOLITION						
Sawcut	LF			\$0.00	\$0.00	\$0.00
Remove Structures and Obstructions	LS	1		\$0.00	\$0.00	\$0.00
Remove Fencing	LF			\$0.00	\$0.00	\$0.00
Remove Structural Concrete	CY			\$0.00	\$0.00	\$0.00
Remove Asphaltic Concrete Pavement				\$0.00	\$0.00	\$0.00
Remove Concrete Sidewalks, Slabs				\$0.00	\$0.00	\$0.00
HAZARDOUS MATERIALS ABATEMENT (If applicable; include heavy metals & asbestos; 5% of construction cost) <i>Enter \$0 in Unit Price column if none required.</i>	LS	1		\$0.00	\$0.00	\$0.00
UTILITY RELOCATION (If necessary) Only the cost of utilities needing relocation as a direct result of the enhancement project are eligible for federal reimbursement. Because of the costs involved, the undergrounding of overhead utilities is not eligible	LS	1		\$0.00	\$0.00	\$0.00
RETAINING WALL (Concrete; SF of face above the footing)	SFF			\$0.00	\$0.00	\$0.00
EARTHWORK						
General Excavation	CY			\$0.00	\$0.00	\$0.00
Drainage Excavation				\$0.00	\$0.00	\$0.00
Structural Excavation				\$0.00	\$0.00	\$0.00
Structural Backfill				\$0.00	\$0.00	\$0.00
Borrow (In Place)				\$0.00	\$0.00	\$0.00
CURB & GUTTER	LF			\$0.00	\$0.00	\$0.00
AGGREGATE BASE	CY	1,700	\$100.00	\$170,000.00	\$160,310.00	\$9,690.00
PATHWAY OR SIDEWALK MATERIALS						
Concrete	SF			\$0.00	\$0.00	\$0.00
Colored Concrete				\$0.00	\$0.00	\$0.00
Stamped Color Concrete				\$0.00	\$0.00	\$0.00
Precast Concrete Pavers				\$0.00	\$0.00	\$0.00
Asphaltic Concrete	Ton	1,700	\$200.00	\$340,000.00	\$320,620.00	\$19,380.00
Polymer or Resin Stabilized Surface	SF			\$0.00	\$0.00	\$0.00

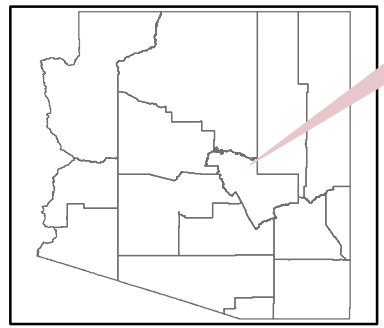
ITEM DESCRIPTION	UNIT	QUAN.	UNIT PRICE	TOTAL	FEDERAL FUNDS @ 94.3%	SPONSOR MATCHING FUNDS @ 5.7%
CROSSWALK ENHANCEMENT						
Concrete Pavers	SF			\$0.00	\$0.00	\$0.00
Stamped Asphalt				\$0.00	\$0.00	\$0.00
Stamped Concrete				\$0.00	\$0.00	\$0.00
Concrete				\$0.00	\$0.00	\$0.00
Integral Color Concrete				\$0.00	\$0.00	\$0.00
PEDESTRIAN ADA RAMP	SF			\$0.00	\$0.00	\$0.00
CULVERT EXTENSIONS	LF			\$0.00	\$0.00	\$0.00
PEDESTRIAN LIGHTING (Includes conduit and trenching) Street lighting is not eligible for federal reimbursement.	Each			\$0.00	\$0.00	\$0.00
HANDRAIL						
Standard	LF			\$0.00	\$0.00	\$0.00
Decorative				\$0.00	\$0.00	\$0.00
SUBTOTAL - SITE ACQUISITION & HARDSCAPE CONSTRUCTION				\$ 570,000	\$537,510	\$32,490
LANDSCAPING & IRRIGATION ITEMS						
TREES (Above 15 gallon in size as required per local code or special design requirements)	Each			\$0.00	\$0.00	\$0.00
TREES (15 GALLON SIZE)	Each			\$0.00	\$0.00	\$0.00
TREES (5 GALLON SIZE)	Each			\$0.00	\$0.00	\$0.00
SHRUBS (5 GALLON SIZE)	Each			\$0.00	\$0.00	\$0.00
SHRUBS (1 GALLON SIZE)	Each			\$0.00	\$0.00	\$0.00
CACTUS (5 GALLON SIZE)	Each			\$0.00	\$0.00	\$0.00
MULCH						
Decomposed Granite	CY			\$0.00	\$0.00	\$0.00
Organic				\$0.00	\$0.00	\$0.00
TOPSOIL	CY			\$0.00	\$0.00	\$0.00
SEEDING	Acre			\$0.00	\$0.00	\$0.00
TURF SOD	SY			\$0.00	\$0.00	\$0.00
BOULDERS	Each			\$0.00	\$0.00	\$0.00
IRRIGATION SYSTEM						
Drip	SF			\$0.00	\$0.00	\$0.00
Turf				\$0.00	\$0.00	\$0.00
SLEEVEING FOR IRRIGATION SYSTEM						
Directional Bore	LF			\$0.00	\$0.00	\$0.00
Cut and Patch				\$0.00	\$0.00	\$0.00
LANDSCAPE HEADER CURB	LF			\$0.00	\$0.00	\$0.00
LANDSCAPE ESTABLISHMENT (Typically 4.5% of the cost of landscaping)	LS			\$0.00	\$0.00	\$0.00

ITEM DESCRIPTION	UNIT	QUAN.	UNIT PRICE	TOTAL	FEDERAL FUNDS @ 94.3%	SPONSOR MATCHING FUNDS @ 5.7%
SUBTOTAL – LANDSCAPING & IRRIGATION ITEMS				\$ -	\$0	\$0
SITE FURNISHINGS						
BENCHES	Each			\$0.00	\$0.00	\$0.00
SEATWALLS	LF			\$0.00	\$0.00	\$0.00
BIKE RACKS	Each			\$0.00	\$0.00	\$0.00
TRASH RECEPTACLES	Each			\$0.00	\$0.00	\$0.00
DRINKING FOUNTAINS	Each			\$0.00	\$0.00	\$0.00
SIGNAGE (Standard Traffic Control)	Each			\$0.00	\$0.00	\$0.00
TREE GRATES	Each			\$0.00	\$0.00	\$0.00
SUBTOTAL – SITE FURNISHINGS				\$ -	\$0	\$0
OTHER CONSTRUCTION ITEMS (List line items)						
				\$0.00	\$0.00	\$0.00
				\$0.00	\$0.00	\$0.00
				\$0.00	\$0.00	\$0.00
				\$0.00	\$0.00	\$0.00
				\$0.00	\$0.00	\$0.00
				\$0.00	\$0.00	\$0.00
				\$0.00	\$0.00	\$0.00
				\$0.00	\$0.00	\$0.00
SUBTOTAL - OTHER CONSTRUCTION LINE ITEMS				\$ -	\$0	\$0
MOBILIZATION AND ADMINISTRATION COSTS						
CONTRACTOR MOBILIZATION (Typically 8% of construction cost)	LS	1	\$40,000.00	\$40,000.00	\$37,720.00	\$2,280.00
TRAFFIC CONTROL (0-8% of construction cost)	LS	1	\$40,000.00	\$40,000.00	\$37,720.00	\$2,280.00
CONSTRUCTION SURVEY & LAYOUT (Typically 1% of construction cost)	LS	1	\$8,000.00	\$8,000.00	\$7,544.00	\$456.00
CONSTRUCTION CONTINGENCIES (Typically 5% of construction cost)	LS	1	\$30,000.00	\$30,000.00	\$28,290.00	\$1,710.00
CONSTRUCTION ADMINISTRATION (Averaging 18% of construction cost)	LS	1	\$25,000.00	\$25,000.00	\$23,575.00	\$1,425.00
SUBTOTAL – MOBILIZATION & ADMINISTRATION COSTS				\$ 143,000	\$134,849.00	\$8,151.00
TOTAL STAGE V COSTS (CONSTRUCTION) (Enter this amount in Box A below.)				\$ 713,000	\$672,359.00	\$40,641.00
ADOT REVIEW FEES (Cannot be applied to the federal participation or the local match. On local Certification Acceptance or Self-administration projects, change to \$3,000)	LS	1	\$25,000.00	\$25,000.00	NO ENTRY	
TOTAL PROJECT COST (All subtotals + ADOT review fee)				\$ 1,210,000	NO ENTRY	

ITEM DESCRIPTION	UNIT	QUAN.	UNIT PRICE	TOTAL	FEDERAL FUNDS @ 94.3%	SPONSOR MATCHING FUNDS @ 5.7%
SUMMARY OF FEDERAL AND LOCAL FUNDS						
TOTAL STAGE V COSTS (CONSTRUCTION) FROM THE ESTIMATE ABOVE, AND DESIGN COSTS IF REQUESTING FEDERAL FUNDS FOR DESIGN. Include design costs (Stages II thru IV) if federal funds are requested for design as shown under Design Costs in the federal column above.					BOX A	\$ 713,000
TOTAL <u>FEDERAL FUNDS</u> CAPPED @ 94.3% (.943 x amount shown in Box A above). <i>Note: For local projects, the maximum federal funds that can be requested is \$500,000 (\$1,000,000 for state projects).</i>					BOX B	\$ 672,359
TOTAL SPONSOR <u>MATCHING FUNDS</u> (.057 x cost shown in Box A above). <i>Note: The maximum amount that should be shown on this line is \$30,223 for local projects (\$60,445 for state projects).</i>					BOX C	\$ 40,641
TOTAL SPONSOR <u>ADDITIONAL FUNDS</u> (OVERMATCH). Enter the amount in Box A in excess, if any, of \$530,223 for local projects or \$1,060,445 for state projects.					BOX D	\$ -
<u>TOTAL SPONSOR FUNDS</u> (Sum of Box C and Box D).					BOX E	\$ 40,641



Young Road Paving Project Project Limits



Project Location

SUPERIOR APPLICATION



CAG's Rural Transportation Advocacy Council

Priority Project List – FY25

APPLICATION

GENERAL PROJECT INFORMATION					
SPONSORING AGENCY:	Town of Superior	DATE SUBMITTED:	11/16/2023		
CONTACT NAME:	Lana Clark	TITLE:	Engineer		
EMAIL ADDRESS:	sclark@superioraz.gov	PHONE #:	520-689-5752		
<input checked="" type="checkbox"/> ROADWAY IMPROVEMENT	Roadway Name:	Main Street			
	Starting Location:	33.171070, -111.064845			
	Ending Location:	33.174841, -111.054179			
	Length (to the 0.1 of a mile):	1.24			
	# of Lanes (Before & After):	Before:	2	After:	2
<input checked="" type="checkbox"/> INTERSECTION IMPROVEMENT	Roadway Name "A":	N Magma Ave			
	Roadway Name "B":	N Pinal Ave			
<input type="checkbox"/> BRIDGE IMPROVEMENT	<input type="checkbox"/> Restoration/Operational <input type="checkbox"/> Replacement <input type="checkbox"/> Widening	Bridge Sufficiency Rating (LINK to ADOT NBI Table)			
		Structurally Deficient?	<input type="checkbox"/> Yes	<input type="checkbox"/> No	
		Functionally Obsolete?	<input type="checkbox"/> Yes	<input type="checkbox"/> No	
<input checked="" type="checkbox"/> OTHER	Description of project type:	1. Striping center line, stop line, stop text. 2. Striping Crosswalks, parking lanes, bicycle lanes 3. WCR improvements at the intersections with Pinal Ave, Magma Ave			
FEDERAL FUNCTIONAL CLASSIFICATION (LINK: FEDERAL FUNCTIONAL CLASSIFICATION MAPS):		https://adot.maps.arcgis.com/apps/webappviewer/index.html?id=7910e9ddd68b43f3a5b86aaf19119081			
AVERAGE ANNUAL DAILY TRAFFIC (AADT) COUNT: (LINK: AADT COUNTS): https://adot.maps.arcgis.com/apps/webappviewer/index.html?id=07cac1757f8a4ce1a73a6e5e79563fe4		1250	DATE OF AADT COUNT:		4/22/2020

COST ESTIMATE & PROJECT PROGRAMMING

<input type="checkbox"/> DESIGN	FY Program Year:	2025-2029	
	Funding Source Request:	<input type="checkbox"/> STBGP	<input type="checkbox"/> HURF Exchange
	Other Non-Local Funding Sources to be Utilized:	<input type="checkbox"/> In-Kind Contribution	
	Total Cost Estimate:	0	
	Federal Share (STBGP or HURF Exchange):	0	
	Minimum Required Local Match (STBGP = 5.7%):	0	
	<i>NOTE: HURF Exchange provides 90% of costs up front. The remaining 10% will be reimbursed upon project completion.</i>		
<input checked="" type="checkbox"/> CONSTRUCTION	FY Program Year:		
	Funding Source Request:	<input type="checkbox"/> STBGP	<input checked="" type="checkbox"/> HURF Exchange
	Other Non-Local Funding Sources to be Utilized:	<input type="checkbox"/>	
	Total Cost Estimate:	\$ 1,069,555.0	
	Federal Share (STBGP or HURF Exchange):	\$1,000,000.0	
	Minimum Required Local Match (STBGP = 5.7%):	\$ 69,555.0	
	<i>NOTE: HURF Exchange provides 90% of costs up front. The remaining 10% will be reimbursed upon project completion.</i>		
<p>Please use the "ADOT Cost Estimate Tool" document for your estimate.</p> <p><i>Any application without the required attachment(s) will not be considered for funding.</i></p>			

PROJECT NEED

This section should clearly state why this project is one of the highest priorities within the CAG Region, for which the use of the requested regional funds is the best option (*No more than one page long; Cambria size 10 font*).

PROJECT NEED: The Main street located on the old site of the town and downtown, connecting major and local streets. The street is a Major street that holds up to 3400 traffic daily. Currently, the Street is in bad condition, has inadequate surface and old striping, and cracked and deplorable paving conditions.

The last full-scale avement evaluation for the Town was conducted for the 2008 Superior Small Area Transportation Study. Since this study, multiple roadways have been rehabilitated (mainly the major streets), and others (collector and residential area streets) have deteriorated for various reasons (i.e. lack of maintenance, drainage, weathering, usage of heavy -trucks, etc).

The 2017 Superior Pavement Assessment Study showed that 72.6% of streets within the Town were rated as “Poor” or lower at the time of the assessment. The same study showed that 40% of sidewalks were in poor condition, which needed immediate attention; as a result, system performance is reduced, leading to potentially adverse impacts on quality of life, mobility, travel time, freight movements, and emergency response times.

The Goals of the Town’s transportation system are to improve the mobility of people and goods, protect the natural environment, support economic development, and sustain public support for transportation planning and funding efforts. The town population is projected to increase from 2,906 in 2010 to 4,789 by 2040. Employment is projected to increase from 602 in 2010 to 2,447 by 2040.

The Downtown is growing significantly. The Town supports and provides several events throughout the year, bringing up to 10,000 visitors or more during the event weekend. Moreover, regular weekends bring up to 3,000 visitors from the Arboretum. Downtown has had more businesses open within the last few years, which increases the traffic. The streets that are connected to the Downtown are heavily used during those events.

- ✓ The lack of local transit options makes it challenging for residents to get around Superior and connect to essential services outside of Superior without access to a motor vehicle.
- ✓ Also, Increased truck traffic from the Resolution Copper Mine could have a negative impact on safety and pavement conditions within Superior.
- ✓ The Town is constantly developing design plans to correct the problems; the lack of funds doesn't allow the Town to resolve the issues as quickly as they wish.
- ✓ With the business district being the center of the Town, the community sees a revitalized business district as the core to its economic future.
- ✓ A comprehensive network of paved streets is needed to accommodate increasing travel demands resulting from the expected growth in population and employment.
- ✓ The street pavement rehabilitation projects would release the burden for the community not getting immediate help from police, ambulance, and fire, and would increase the mobility and safety of the public.
- ✓ Sidewalks and bicycle lanes are integral parts of a town’s transportation system. The ability to efficiently and safely carry non-motorized travel within the Town is related directly to the conditions of the pedestrian and bicycle facilities.
- ✓ Additional parking spaces striping, bicycle lanes, pedestrian crosswalks, standard WC ramps, and improved sidewalk conditions would help regulate human traffic during business hours, weekends, and events.
- ✓ The reconstruction of these collector streets nearby the downtown area will provide multimodal facilities, such as crosswalks, improved sidewalks, and bicycle lanes.

PROJECT WORK DESCRIPTION

Provide a brief work description that describes the work to be performed, existing and/or proposed conditions, its benefits and overall cost estimate. *(No more than one page long; Cambria size 10 minimum font)*. **Please ATTACH a Project Vicinity/Project Location Map on a separate page as part of the overall application.**

PROJECT NEED:

The paving and striping of Main street.

The length of the street is about 1.4 miles; the width is 60 feet. The street requires milling/removing the existing 2" of asphalt and paving streets with New 3" rubberized asphalt.

All streets need striping, with parking spaces, bike lanes, and crosswalks.

The sidewalks are to be repaired at parts where concrete is moved or has cracks, with the installation of ADA ramps per standard codes and regulations.

Project Elements:

1. Main Street: New 3-inch Asphalt/ 2" milling remove existing AC
2. Install ADA Handicap Ramps
3. Centerline and fog line striping
4. Crosswalk and stop bars striping
5. Bike lane striping
6. Parking spaces striping.

Engineering costs are In-Kind Match expenses to be provided by Town:

7. The Town of Superior will provide the design and Final As-Built construction plans.
8. Preparation of BID documents per the grant and Town of Superior bidding requirements.
9. Bid tabulation and certification.
10. Meetings & progress reports as required by the grant and Town of Superior.

ITEMS TO BE ADDRESSED	
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PROJECT INCLUSION IN PREVIOUS PLANS	Is the project included in previous plans?		<input checked="" type="checkbox"/> YES	<input type="checkbox"/> NO
	<input checked="" type="checkbox"/>	2015 Regional Transportation Plan (RTP)	<input checked="" type="checkbox"/>	Pre-Scoping Studies
	<input checked="" type="checkbox"/>	Road Safety Assessment (RSA)	<input type="checkbox"/>	Comprehensive Economic Development Strategy (CEDS)
	<input checked="" type="checkbox"/>	Pinal County Comprehensive Plan	<input checked="" type="checkbox"/>	Local Comprehensive Plan / General Plan 2022-2040
	<input checked="" type="checkbox"/>	Local Transportation Plan	<input checked="" type="checkbox"/>	Other #1 Superior Pavement Assessment Study-2017
	<input checked="" type="checkbox"/>	Other #2 TIP- PRTA-2021	<input checked="" type="checkbox"/>	Other #3 2008 Town of Superior Small Area Transportation Study (SATS)
COMMUNITY TRANSPORTATION BENEFITS	Does the project provide multimodal improvements? Yes or No and Why?		Yes. The street needs a flashing crosswalk or similar improvements, updated WC Ramps, and striping for car parking and bicycle lanes at the intersections with major streets that connect to The Public Besich Park and Downtown. Yes. Superior became a widely used tourist attraction place. Approximately 3000 – 3,500 visitors visit the Arboretum and downtown restaurants and shops during the weekends. Many cars, motorcycles, bicycles, and hikers park on Main Street.	
	Does the project provide Community Investments and/or Economic Development benefits? Yes or No and Why?			
SAFETY COUNTERMEASURES <i>(For Potential Use of HSIP Funds)</i>	Can you provide crash data, including fatalities over the last five (5) years? Yes or No? <i>(Cite Source of Crash Data)</i>		Yes. 2017-2021 ADOT crash data report. The 2022 ADOT crash data report is not available yet. Yes.	
	Does the project primarily include any of the 44 safety countermeasures listed on the next page? FHWA safety countermeasures Yes or No?			

SAFETY COUNTERMEASURE		Y or N
1. "Stop Ahead" pavement markings		Y
2. "Vehicles Entering When Flashing" (VEWF) system (advance post mounted signs on major and loops on minor)		
3. 12-inch signal heads all faces all directions		
4. Actuated advance warning dilemma zone protection system		
5. 3-inch yellow retroreflective sheeting to signal backplates		
6. Advance street name signs		
7. All red clearance interval new or existing signals		
8. All-way stop control (with flashing beacons)		
9. All-way stop control (without flashing beacons)		Y
10. Composite shoulders (5 feet minimum) on rural two lane roads		
11. 3-lane roadways with center turn lane		
12. Flashing lights and sound signals at Railroad grade crossings		
13. Gates with signs at railroad at grade crossings		
14. Improve 2-lane roadway to 4-lane divided roadway		
15. Improvements that include reducing 11 feet lanes to 9 feet		
16. Install shoulder rumble strips		Y
17. Install centerline rumble strips		
18. Install wide edgelines (6-inch min)		
19. Install a traffic signal (engineering study demonstrates meeting MUTCD Warrant 7)		
20. Install dynamic signal warning flashers		
21. Install dynamic speed feedback sign at high speed crash curve site with identified speeding problems		
22. Install Intersection Conflict Warning Systems (ICWS) for 4-lane at 2-lane intersections		
23. Install ICWS for 2-lane at 2-lane intersections		
24. Install ICWS with a combination of overhead and advanced post mounted signs (various messages) and flashers		
25. Install ICWS with overhead signs (various messages) and flashers at the intersection on minor; loop on major		
26. Install ICWS with post mounted signs (various messages) and flashers in advance of the intersection on major; loop on major		
27. Modern roundabout where a signalized intersection exists		
28. Roundabout at a high-speed 3 or 4 leg rural intersection		
29. Modify zero or negative left-turn lane offset to create positive offset		
30. New left-turn lanes with positive offset		
31. Pavement friction (Microsurfacing, Open Graded Friction Course, High Friction Surfacing)		
32. Pedestrian Hybrid Beacon (PHB or HAWK)		
33. Position offset left-turn lanes on both major road approaches		
34. Protected only left-turn signal equipment		
35. Protected-permissive left-turn signal equipment		
36. Raised median		
37. Right-turn lane geometry with increased line of sight		
38. Rural 2-lane roads with TWLTL (Two-Way Left Turn Lanes)		
39. Urban 2-lane road with TWLTL		
40. Safety edge treatment on rural highways		
41. Single- or multi-lane roundabout at a 2-way stop-controlled intersection		
42. Single- or multi-lane roundabout at existing signalized intersection		
43. 2-way stop control at uncontrolled neighborhood intersections		Y
44. Wet-reflective pavement markings		Y

OTHER CONSIDERATIONS

(Provide Any Supplemental Supporting Documentation – Optional)

ENVIRONMENTAL	<p>Are there any potential environmental impacts or challenges of the project that you can foresee?</p> <p>Yes or No and Why?</p> <p><i>(e.g. endanger species, cultural assets, hazardous materials sites, 4Fs, Title VI populations, wet lands that would be affected, etc.)</i></p>	No.
RIGHT-OF-WAY (ROW)	<p>Please describe any ROW items associated with this project.</p> <p><i>(e.g. Will ROW be required? How much ROW? Is the State Land Department involved?)</i></p>	No.
DEVELOPMENT ACTIVITY	<p>Is there any planned or ongoing development activity that could impact the proposed project? If Yes, please explain.</p>	No.
UTILITIES	<p>Will the project include/require any utility relocation(s) by the project sponsor? If Yes, please explain.</p>	No.
DRAINAGE	<p>Are there any drainage issues and/or proposed improvements associated with this project?</p>	Yes. Proposed to install Catch Basin at the intersection N Lobb Avenue & Porphyry Street connecting to existing 12" SD line.

LEVEL OF SERVICE (LOS):

Current:

D

After:

A

Level of Service "A" =	Free-flow traffic with individual users virtually unaffected by the presence of others in the traffic stream.
Level of Service "B" =	Stables traffic flow with a high degree of freedom to select speed and operating conditions but with some influence from users.
Level of Service "C" =	Restricted flow that remains stable but with significant interactions with others in the traffic stream. The general level of comfort and convenience declines noticeably at this level.
Level of Service "D" =	High-density flow in which speed and freedom to maneuver are severely restricted and comfort and convenience have declined even though flow remains stable.
Level of Service "E" =	Unstable flow at or near capacity levels with poor levels of comfort and convenience.
Level of Service "F" =	Forced traffic flow in which the amount of traffic approaching a point exceeds the amount that can be served. LOS F is characterized by stop-and-go waves, poor travel times, low comfort and convenience, and increased accident exposure.

HURF Exchange Project Scoping Form

Note: The Project Scoping Form, Project Cost Estimate and Project and Draw Schedule forms (all contained in this file) must be completed and submitted with the project initiation request. Each phase requires advanced authorization by ADOT. HURF Exchange funding may not be combined with federal aid.

INSTRUCTIONS: This form is required to accompany a HURF Exchange Project Initiation request.

Enter information into GREEN CELLS

PROJECT INFORMATION

(information provided in this section will auto-populate to the Project Cost Estimate and Draw Schedule tabs)

Project Sponsor	Town of Superior
Sponsor Contact	Lana Clark
Contact Phone #	520-689-5752
Contact Email	sclark@superioraz.gov
Project Name	Main Street paving and striping
Project Location	Superior, AZ 85173
Functional Classification (select from)	Major Collector
Termini Begin/End	33.171070, -111.064845 / 33.174841, -111.054179
ADOT District (select from list)	Southeast District

ADOT USE ONLY

ADOT Project Number (5 digit)		IGA Number	
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Funding Information

COG/MPO (select from list)				
TIP Information	Description	Design	Right of Way	Construction
	Year Programmed	NA	NA	2025-2026
	TIP Number			
	HURF Exchange Amount	NA	NA	\$1,000,000.00
	Sponsor Amount	NA	NA	\$69,555.00
	Total Amount	\$0.00	\$0.00	\$1,069,555.00

Scope of Work Summary

Delivery Method (select from list)	Use own forces
Scoping document attached? (select from list)	Yes

Major Items of Work (press Alt-Tab to create a new line; press Alt-Tab-Tab to create a new paragraph)	Project Elements:
	1.Main Street: New 3-inch Asphalt/ 2" milling remove existing AC
	2.Install ADA Handicap Ramps
	3.Centerline and fog line striping
	4.Crosswalk and stop bars striping
	5.Bike lane striping
	6.Parking spaces striping.

HURF Exchange Project Cost Estimate

Note: The Project Scoping Form, Project Cost Estimate and Project and Draw Schedule forms (all contained in this file) must be completed and submitted with the project initiation request. Each phase requires advanced authorization by ADOT. HURF Exchange funding may not be combined with federal aid.

INSTRUCTIONS: This form is required to accompany a HURF Exchange Project Initiation request. List all items necessary to develop and construct the project. The sponsoring agency is responsible for verifying all costs and their accuracy. Construction cost overruns will be the responsibility of the sponsoring agency.

**Enter values
into GREEN
CELLS**

PROJECT INFORMATION

(fields below will be populated based on information entered on the Project Scoping Form tab)

Project Sponsor	0
Sponsor Contact	0
Contact Phone #	0
Contact Email	0
Project Name	0
Project Location	0
Termini Begin/End	0
COG/MPO	0
Design TIP Number	0
Right of Way TIP Number	0
Construction TIP Number	0

ADOT USE ONLY

ADOT Project Number

0

STAGE I – SCOPING (15% Preliminary Design)

NOT ELIGIBLE FOR HURF EXCHANGE FUNDING

STAGES II, III, IV and V - DESIGN**DESIGN COSTS**

ITEM DESCRIPTION	UNIT	QUANTITY	UNIT PRICE	TOTAL
PS&E's - Plans, Special Provisions, Cost Estimates & Schedules (10%-20% of construction cost.)	Lump Sum	1		\$0.00
GEOTECHNICAL INVESTIGATION (If a report is necessary, anticipate 5% of construction cost) Includes testing, Geotech Report, Materials & Pavement Design Report) Enter \$0 in Unit Price column if none required.	Lump Sum	1		\$0.00
DRAINAGE REPORT (If a report is necessary, anticipate 5% of construction cost) Enter \$0 in Unit Price column if none required)	Lump Sum	1		\$0.00
STORM WATER POLLUTION PREVENTION PLAN (Required if there is over 1 acre of total disturbance, 1% of construction cost) Enter \$0 in Unit Price column if none required.	Lump Sum	1		\$0.00

SUBTOTAL – PROJECT DESIGN COSTS**\$0.00****RIGHT OF WAY COSTS**

ITEM DESCRIPTION	UNIT	QUANTITY	UNIT PRICE	TOTAL
RIGHT OF WAY , Costs for pre-acquisition activities (plans, title reports, appraisals, etc)	Lump Sum	1		\$0.00
RIGHT-OF-WAY ACQUISITION (if necessary)	Lump Sum	1		\$0.00

SUBTOTAL – RIGHT OF WAY COSTS**\$0.00**

MAIN STREET, SUPERIOR

STAGE V – CONSTRUCTION				
SITE ACQUISITION & HARDSCAPE CONSTRUCTION				
ITEM DESCRIPTION	UNIT	QUANTITY	UNIT PRICE	TOTAL
INSTALLATION OF STORMWATER POLLUTION PREVENTION MEASURES (If over 1 acre of disturbance, 5% of construction costs) <i>Enter \$0 in Unit Price column if area of disturbance is less than one acre.</i>	Lump Sum	1		\$0.00
SITE PREPARATION (Clearing and grubbing, plant salvage)	Lump Sum	1	\$30,000.00	\$30,000.00
DEMOLITION				
Sawcut	Linear Foot	1,373	\$35.00	\$48,055.00
Remove Structures and Obstructions	Lump Sum	1		\$0.00
Remove Fencing	Linear Foot			\$0.00
Remove Structural Concrete				\$0.00
Remove Asphaltic Concrete Pavement	Cubic Yard			\$0.00
Remove Concrete Sidewalks, Slabs				\$0.00
HAZARDOUS MATERIALS ABATEMENT (If applicable; include heavy metals & asbestos; 5% of construction cost) <i>Enter \$0 in Unit Price column if none required.</i>	Lump Sum	1		\$0.00
UTILITY RELOCATION (If necessary) Only the cost of utilities needing relocation as a direct result of the HURF Exchange project is eligible for HURF Exchange.	Lump Sum	1		\$0.00
RETAINING WALL (Concrete; SF of face above the footing)	Square Footage Facing			\$0.00
EARTHWORK				
General Excavation				\$0.00
Drainage Excavation				\$0.00
Structural Excavation	Cubic Yard			\$0.00
Structural Backfill				\$0.00
Borrow (In Place)				\$0.00
CURB & GUTTER	Linear Foot			\$0.00
ROADWAY/PAVING				
Milling	Square Yards	55,000	\$7.00	\$385,000.00
Paving	Tons	5,550	\$45.00	\$249,750.00
AGGREGATE BASE	Cubic Yard			\$0.00
PATHWAY OR SIDEWALK MATERIALS				
Concrete				\$0.00
Colored Concrete	Square Foot			\$0.00
Stamped Color Concrete				\$0.00
Precast Concrete Pavers				\$0.00
Asphaltic Concrete	Tons			\$0.00
Polymer or Resin Stabilized Surface	Square Foot			\$0.00
CROSSWALK ENHANCEMENT				
Concrete Pavers				\$0.00
Stamped Asphalt	Square Foot			\$0.00
Stamped Concrete				\$0.00
Concrete				\$0.00
Integral Color Concrete				\$0.00
PEDESTRIAN ADA RAMP	Square Foot	120	\$200.00	\$24,000.00
CULVERT EXTENSIONS	Linear Foot			\$0.00
SUBTOTAL - SITE ACQUISITION & HARDSCAPE CONSTRUCTION				\$736,805.00

MAIN STREET, SUPERIOR

OTHER CONSTRUCTION ITEMS (List line items)				
ITEM DESCRIPTION	UNIT (Lump Sum, Ton, etc.)	QUANTITY	UNIT PRICE	TOTAL
TRAFFIC- Signing & Pavement Markings (15%)	LS	1	\$115,000.00	\$115,000.00
				\$0.00
				\$0.00
				\$0.00
				\$0.00
				\$0.00
				\$0.00
				\$0.00
SUBTOTAL - OTHER CONSTRUCTION LINE ITEMS				\$115,000.00
MOBILIZATION AND ADMINISTRATION COSTS				
ITEM DESCRIPTION	UNIT	QUANTITY	UNIT PRICE	TOTAL
CONTRACTOR MOBILIZATION (Typically 8% of construction cost)	Lump Sum	1	\$65,800.00	\$65,800.00
TRAFFIC CONTROL (0-8% of construction cost)	Lump Sum	1	\$65,800.00	\$65,800.00
CONSTRUCTION SURVEY & LAYOUT (Typically 1% of construction cost)	Lump Sum	1		\$0.00
CONSTRUCTION CONTINGENCIES (Typically 5% of construction cost)	Lump Sum	1	\$41,150.00	\$41,150.00
CONSTRUCTION ADMINISTRATION Typically 20% of construction cost)	Lump Sum	1	\$45,000.00	\$45,000.00
SUBTOTAL – MOBILIZATION & ADMINISTRATION COSTS				\$217,750.00
TOTAL STAGE V COSTS (CONSTRUCTION)				\$1,069,555.00
TOTAL PROJECT COST				\$1,069,555.00
SUMMARY OF HURF EXCHANGE AND SPONSOR FUNDS				
The data below is automatically calculated based on the information entered above and the amount programmed in the TIP as entered on the Project Scoping Form.				
PHASE	HURF EXCHANGE FUNDS IN TIP	SPONSOR FUNDS	TOTAL	
TOTAL DESIGN	\$0.00	\$0.00	\$0.00	
TOTAL RIGHT OF WAY	\$0.00	\$0.00	\$0.00	
TOTAL CONSTRUCTION	\$0.00	\$1,069,555.00	\$1,069,555.00	
TOTALS	\$0.00	\$1,069,555.00	\$1,069,555.00	

Prepared by:

Name Lana Clark

Title Engineer

Company Town of Superior

Phone 520-689-5752

Email sclark@superioraz.gov

HURF Exchange Project and Draw Schedules

Note: The Project Scoping Form, Project Cost Estimate and Project and Draw Schedule forms (all contained in this file) must be completed and submitted with the project initiation request. Each phase requires advanced authorization by ADOT. HURF Exchange funding may not be combined with federal aid.

INSTRUCTIONS: This form is required to accompany a HURF Exchange Project Initiation request. Enter the Estimated Completion Dates as requested for each Stage of Development and Construction. Also enter the Expected Draw Dates for these phases. Draw amounts for each phase are auto-calculated based on the amount programmed in the TIP as entered on the Project Scoping Form.

**Enter dates into
GREEN CELLS**

Project Information

(fields below will be populated based on information entered on the Project Scoping Form tab)

Project Sponsor	Town of Superior
Sponsor Contact	Lana Clark
Contact Phone #	520-689-5752
Contact Email	sclark@superioraz.gov
Project Name	Main Street paving and striping
Project Location	Superior, AZ 85173
Termini Begin/End	33.171070, -111.064845 / 33.174841, -111.054179
COG/MPO	0
Design TIP Number	0
Right of Way TIP Number	0
Construction TIP Number	0

ADOT USE ONLY

ADOT Project Number

0

Project Development

Development Schedule		HURF Exchange Funding Draw Schedule		
Stage	Estimated Completion Date	Draw %	Amount	Expected Draw Date
Project Initiation	7/18/2005	30%	\$ -	
IGA Executed		30%	\$ -	
Request for Authorization to ADOT		30%	\$ -	
Authorization/Start of Work Phase		10%	\$ -	
Stage II		Total	\$ -	
Stage III				
Stage IV				
Bid Ready				

Right of Way

Acquisition Schedule		HURF Exchange Funding Draw Schedule		
Activity	Estimated Completion Date	Draw %	Amount	Expected Draw Date
Request for Authorization to ADOT		30%	\$ -	
Authorization/Start of Work Phase		30%	\$ -	
All Parcels Acquired By		30%	\$ -	
		10%	\$ -	
		Total	\$ -	

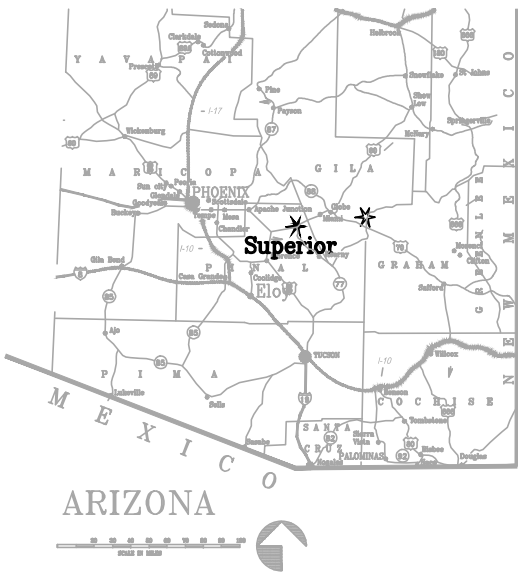
Construction

Construction Schedule		HURF Exchange Funding Draw Schedule		
Activity	Estimated Completion Date	Draw %	Amount	Expected Draw Date
Request for Authorization to ADOT	1/4/2026	30%	\$ 300,000.00	4/1/2026
Authorization/Start of Work Phase	1/15/2026	30%	\$ 300,000.00	4/10/2026
Bid Advertisement	1/16/2026	30%	\$ 300,000.00	4/20/2026
Bid Opening	2/15/2026	10%	\$ 100,000.00	6/10/2026
Bid Award	2/28/2026	Total	\$ 1,000,000.00	
Notice to Proceed to Contractor	1/10/2026			
Substantial Completion	4/30/2026			
Final Project Walk-through	5/1/2026			

Final Acceptance	5/10/2026
------------------	-----------

* The Final 10% is reimbursed to the Sponsor as follows:

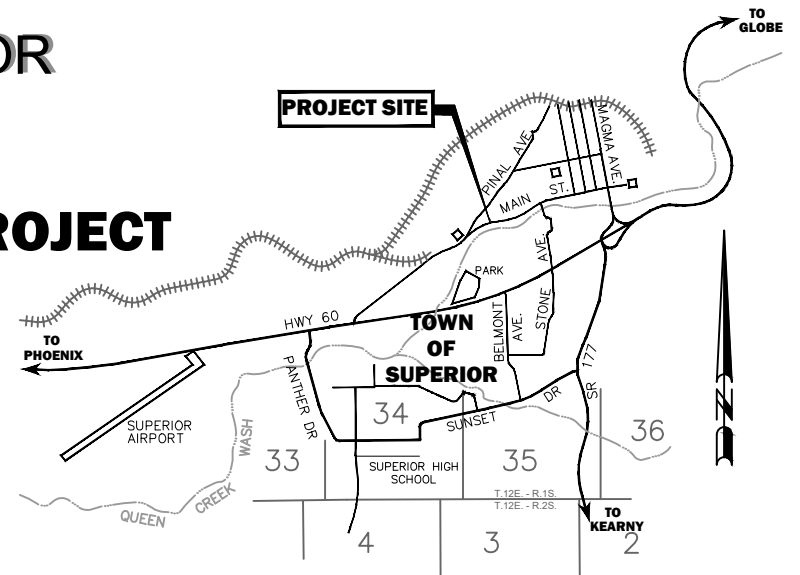
- ▶ For projects involving multiple phases funded with HURF Exchange - the final 10% for each phase, except for the last, will be reimbursed within 30 days of the receipt and approval of an invoice and documentation demonstrating the phase is complete.
- ▶ At final Project completion - the final 10% will be reimbursed upon completion of the project final voucher by ADOT.



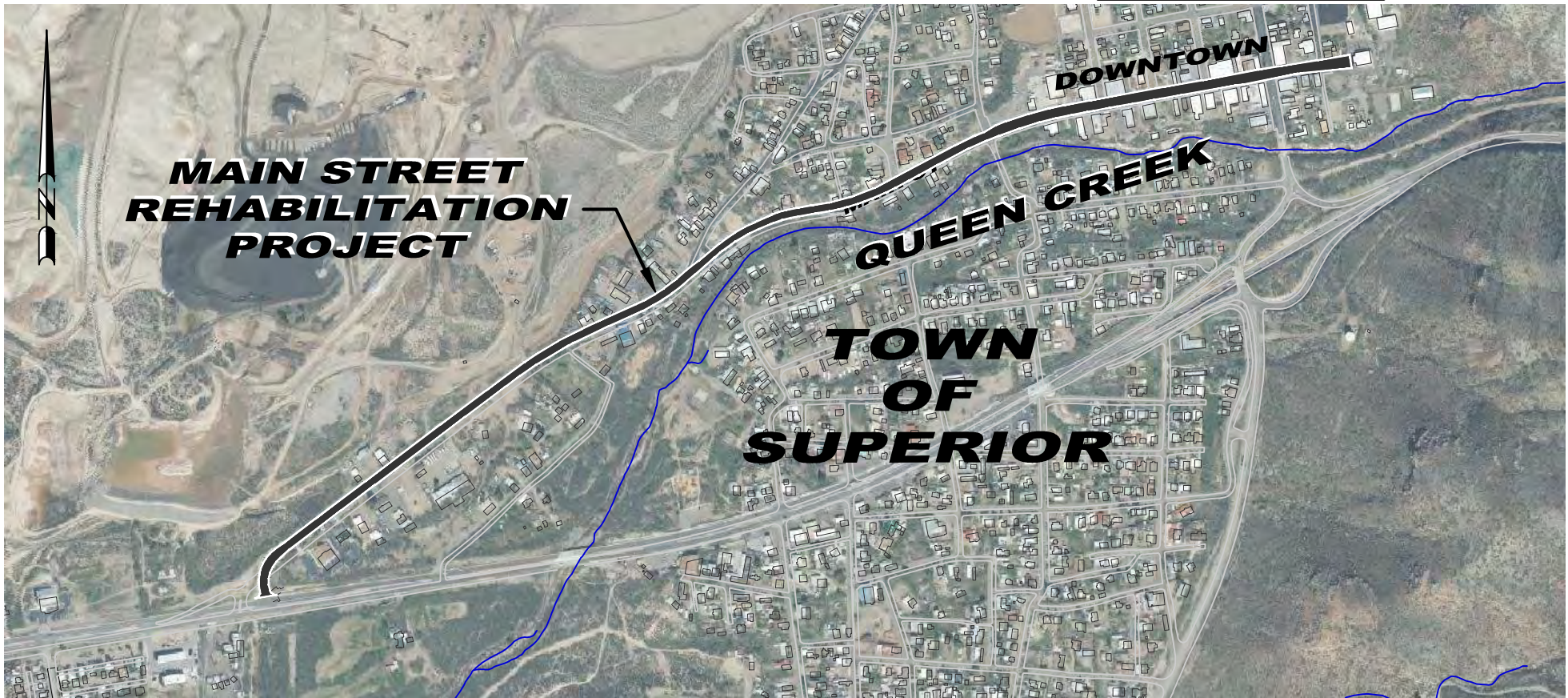
TOWN OF SUPERIOR

PINAL COUNTY, ARIZONA

MAIN STREET PAVING & STRIPING PROJECT



VICINITY MAP

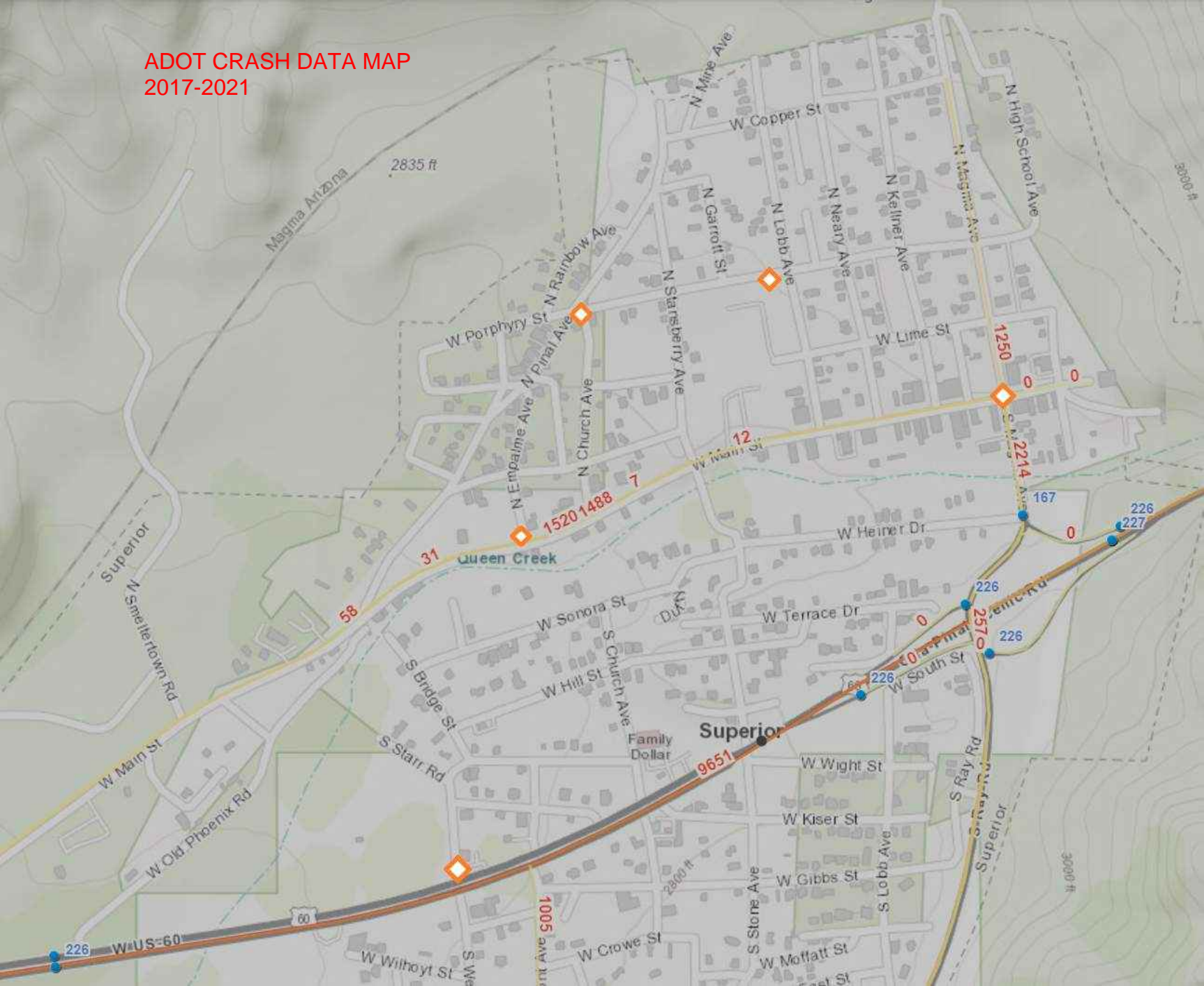


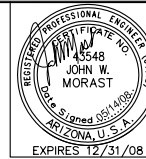
LOCATION MAP


ADOT (ALISS) DATABASE FOR PERIOD 2017-2021

ADOT Crash Data Report - 2017								
3224968	5/4/2017 12:03	2017	U 060	M226	949579.899	833804.3701	U 060	M226
3277202	8/29/2017 19:36	2017	11 LOBB AVE	Porphyry St	949673.5201	835978.6007	11 LOBB AVE	11 PORPHYRY ST
ADOT Crash Data Report - 2018								
3362703	4/20/2018 11:08	2018	11 CHURCH	Moffatt St	949248.2897	832601.3884	11 CHURCH AVE	11 MOFFATT ST
3377663	5/24/2018 23:48	2018	U 060	M224	940011.6224	831706.105	U 060	M224
ADOT Crash Data Report - 2019								
3493191	3/24/2019 15:20	2019	11 MAIN	Empalme St	948143.2	834647.1311	11 MAIN ST	11 EMPALME ST
3515409	5/23/2019 9:15	2019	U 060	M224	939176.0704	831580.3733	U 060	M224
3529531	6/13/2019 11:40	2019	11 PINAL	Porphyry St	948664.8189	835791.4959	11 PINAL AVE	11 PORPHYRY ST
3568722	10/10/2019 0:47	2019	U 060	M224	939698.7018	831654.8713	U 060	M224
3572444	10/18/2019 18:25	2019	U 060	M227	951657.3896	834996.1007	U 060	M227
3576434	10/17/2019 0:50	2019	11 MAIN	Magma Ave	950462.7216	835367.9294	11 MAIN ST	11 MAGMA AVE
ADOT Crash Data Report - 2020								
3666631	3/5/2020 12:47	2020	U 060	Western Ave	33.28751972	-111.1047151	U 060	11 WESTERN AVE
3708842	11/15/2020 15:08	2020	S 177	M167	33.28347364	-111.0984369	S 177	M167
ADOT Crash Data Report - 2021								
3732530	2/6/2021 14:42	2021	U 060	M226	33.28742455	-111.1050878	U 060	M226
3747600	3/24/2021 14:29	2021	U 060	M225	33.28576687	-111.1166348	U 060	M225
3757632	4/20/2021 10:25	2021	U 060	M225	33.28576633	-111.1166389	U 060	M225

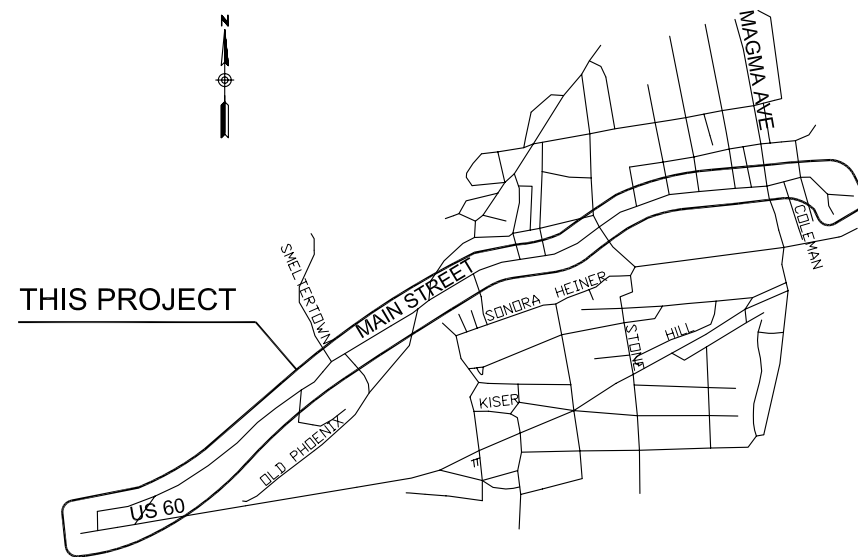
ADOT CRASH DATA MAP 2017-2021





F.A.W.A. REGION	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS	AS BUILT
9	ARIZ.		1	22	
DATE		CONSULTING ENGINEER			
DESIGN:		DRAWN:		CHECKED:	
		the CK Group, Inc. CIVIL • TRANSPORTATION ENGINEERS 16448 N. 40th Street, Suite A Phoenix, Ariz. 85032			

TOWN OF SUPERIOR
PUBLIC WORKS DEPARTMENT
TOWN OF SUPERIOR, AZ.



VICINITY MAP
NTS



2008

MAIN STREET IMPROVEMENTS
FROM US60 HIGHWAY TO PINAL
COUNTY ADMINISTRATION OFFICE

TOWN COUNCIL MEMBERS

MICHAEL ONG HING — MAYOR
SOYLA PERALTA
LYNN HEGLIE
OLGA D. LOPEZ
JAYME VALENZUELA
ROBERT CERVANTES

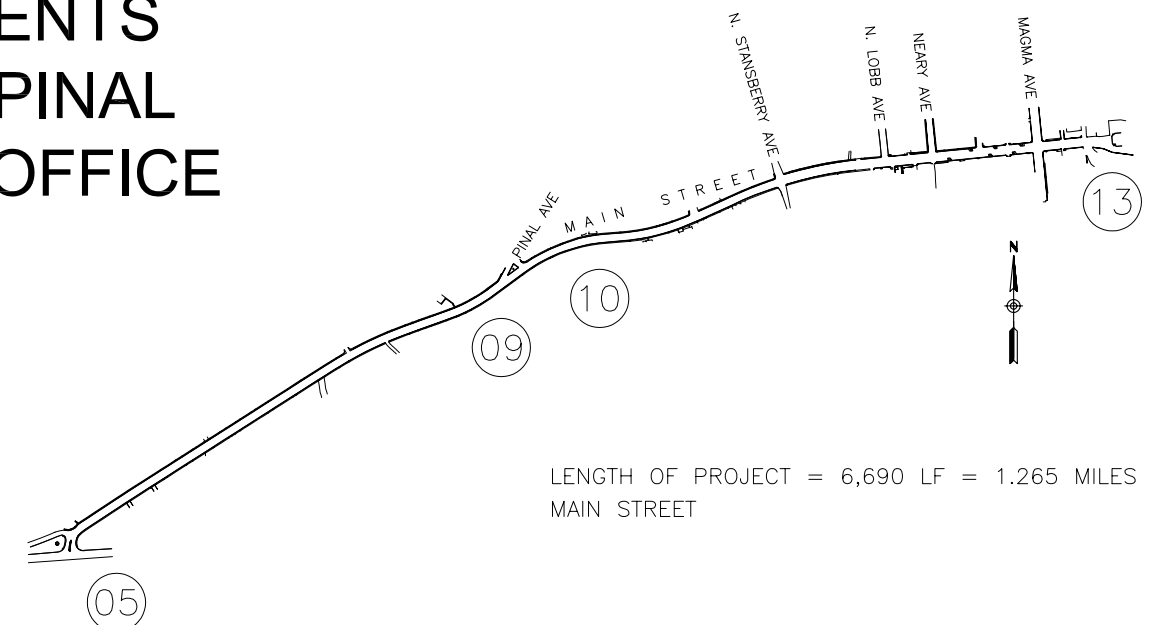
PUBLIC WORKS SUPERVISOR

REBECCA BROTHERS

INDEX OF SHEETS

1	COVER SHEET
2	LEGEND AND NOTES SHEET
3	QUANTITY SUMMARY SHEET
4	TYPICAL SECTIONS
5-13	PAVING PLANS
14-17	INTERSECTION IMPROVEMENTS
18	SIGNING AND STRIPING GENERAL NOTES
19-22	SIGNING AND STRIPING PLANS

KEY MAP



LENGTH OF PROJECT = 6,690 LF = 1.265 MILES
MAIN STREET

APPROVED _____
PUBLIC WORKS DIRECTOR

DATE _____

FROM US60 HIGHWAY TO PINAL
COUNTY ADMINISTRATION OFFICE

	BENCHMARK
	NEW SURVEY MONUMENT
	NEW SURVEY MONUMENT
	COMBINED CURB & GUTTER
	BACKFILL TYPE & SQ. YD. PVMT. REPLACEMENT
	EXISTING CONCRETE PAVEMENT OR SIDEWALK
	NEW CONCRETE SIDEWALK
	NEW CONCRETE DRIVEWAY OR ALLEY ENT. PER DETAIL NO. ON PLANS
	NEW CONCRETE SIDEWALK RAMP PER DETAIL ON PLANS
	AVERAGE GROUND ELEVATION AT R/W LINE
	BACKFILL COMPACTION TYPE
	EXISTING DITCH
	EXISTING IRRIGATION LINE W/SIZE
	EXISTING IRRIGATION STRUCTURE
	EXISTING IRRIGATION STANDPIPE
	NEW IRRIGATION STANDPIPE
	NEW IRRIGATION VALVE
	IRRIGATION BERM
	NEW IRRIGATION STRUCTURE PER DETAIL ON PLANS
	EXISTING WATER LINE W/SIZE & TYPE (12" AND SMALLER)
	EXISTING WATER LINE W/SIZE & TYPE (GREATER THAN 12")
	ADJUST EXIST. WATER VALVE BOX
	WATER SERVICE W/SIZE AND WATER METER BOX
	EXISTING FIRE HYDRANT
	NEW OR RELOCATED FIRE HYDRANT BY CONTRACTOR
	EXISTING WATER VALVE W/TOP OF OPERATING NUT ELEVATION
	EXISTING SANITARY SEWER LINE W/SIZE & TYPE (12" AND SMALLER)
	EXISTING SANITARY SEWER LINE W/SIZE & TYPE (GREATER THAN 12")
	EXISTING MANHOLE
	NEW MANHOLE
	CATCH BASIN, GUTTER INLET (LENGTH TO SCALE)
	CATCH BASIN, CURB INLET (LENGTH TO SCALE)
	CATCH BASIN, CURB & GUTTER INLET (LENGTH TO SCALE)
	EXISTING STORM DRAIN LINE W/SIZE & TYPE (GREATER THAN 12")
	NEW PIPE FOR STORM DRAIN OR IRRIGATION LINE
	EXISTING GAS LINE W/SIZE
	EXISTING UNDERGROUND ELECTRIC CABLE OR SINGLE CONDUIT
	EXISTING UNDERGROUND ELECTRIC DUCT (SPECIFY NUMBER)
	EXISTING TELEPHONE BURIED CABLE OR SINGLE CONDUIT
	EXISTING UNDERGROUND TELEPHONE DUCT (SPECIFY NUMBER)
	EXISTING UNDERGROUND CABLE TV
	EXISTING UNDERGROUND FIBER OPTIC LINE
	EXISTING TRAFFIC SIGNAL POLE W/MAST ARM & SIGNAL INDICATIONS
	EXISTING STREET OR TRAFFIC SIGN
	EXISTING UTILITY POLE W/LINE INDICATING WIRE DIRECTION
	EXISTING WIRE FENCE
	EXISTING BLOCK FENCE
	EXISTING WOOD FENCE
	MAIL BOX
	EXISTING POWER POLE DOWN GUY ANCHOR
	EXISTING STREET LIGHT & POLE
	EXISTING TREE OR STUMP TO BE REMOVED - MORE THAN 12" DIA.
	EXISTING TREE TO BE TRANSPLANTED BY CONTRACTOR
	EASEMENT LINE
	EXISTING OR NEW R/W LINE
	PAVEMENT CENTER LINE OR MONUMENT LINE
	EXISTING TREE TO BE REMOVED (NON PAY ITEM)
	EXISTING TREE TO REMAIN
	CURB OPENING INLET
	GRATE INLET

THE SPACE BETWEEN THE BACK OF NEW DRIVEWAY ENTRANCES AND EXISTING A.C. DRIVEWAYS SHALL BE FILLED WITH A MINIMUM OF 3" A.C.S.C. ON 100% COMPACTED NATIVE SOIL. WHERE EXISTING PAVEMENT AND BASE THICKNESS EXCEED THE MINIMUMS, MATCH THE EXISTING.

THE SPACE BETWEEN THE BACK OF NEW SIDEWALKS AND EXISTING PRIVATE SIDEWALKS, AND THE SPACE BETWEEN THE BACK OF NEW DRIVEWAY ENTRANCES AND EXISTING P.C.C. DRIVEWAYS SHALL BE FILLED WITH P.C.C.. THE THICKNESS AND CLASS SHALL MATCH THAT OF THE NEW SIDEWALK OR DRIVEWAY ENTRANCE.

UNLESS OTHERWISE PROVIDED ON THE PLANS, THE SPACE BETWEEN THE BACK OF THE NEW SIDEWALKS, AND EXISTING A.C. PARKING LOTS, WHICH FALL WITHIN OR ABUT THE R/W SHALL BE FILLED WITH 2" A.C.S.C. ON 100% COMPACTED NATIVE SOIL.

EXISTING IRRIGATION BERMS DISTURBED BY NEW CONSTRUCTION SHALL BE
RECONSTRUCTED AS SHOWN ON PLANS.

ALLEY ENTRANCES, WHICH ARE INDICATED TO BE MODIFIED ON THESE PLANS SHALL BE CONSTRUCTED AND THE SLAB DRIVEWAYS SHALL BE 5" THICK.

CONTRACTOR IS RESPONSIBLE TO ADJUST THE MANHOLE COVER ELEVATION OF WATER VALVE, GAS VALVE AND SEWER MANHOLE TO MATCH THE FINISH GRADE.

"CLEAR" DIMENSIONS FOR DEPTH OF REINFORCING STEEL ARE FROM FACE OF CONCRETE TO FACE OF BARS.

ALL EXPOSED EDGES OF CONCRETE SHALL BE BEVELED OR ROUNDED.

ALL EXPOSED CONCRETE SURFACES SHALL BE FINISHED IN ACCORDANCE WITH ARIZONA DEPT. OF TRANSPORTATION STANDARD SPECIFICATION 601-3.05 - FINISHING FORMED CONCRETE, UNLESS OTHERWISE SPECIFIED.

ALL CONSTRUCTION SHALL BE PERFORMED IN ACCORDANCE WITH CONTRACT SPECIAL PROVISIONS AND DETAILS, PINAL COUNTY STANDARD SPECIFICATIONS AND DETAILS, AT THE TIME OF CONSTRUCTION BID.

ALL STORM SEWER MANHOLES ARE TO BE CONSTRUCTED WITHOUT STEPS.

PIPE CONNECTIONS TO EXISTING CONCRETE PIPE MAINS SHALL BE MADE IN ACCORDANCE WITH DETAILS CALLED OUT ON THE PLANS. CONNECTION TO MAINS SHALL NOT BE CLOSER THAN 5', CENTER TO CENTER.

PIPE CONNECTIONS TO NEW PRECAST CONCRETE PIPE MAINS SHALL BE MADE WITH FACTORY MADE WYES OR TEES. THE DETAIL OF THE FITTINGS MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL PRIOR TO MANUFACTURE.

CATCH BASIN CONNECTOR PIPES SHALL BE LAID ON A STRAIGHT ALIGNMENT AND SLOPE UNLESS OTHERWISE SPECIFIED. IF BREAKS IN ALIGNMENT OR SLOPE ARE NECESSARY TO MEET FIELD CONDITIONS, THE MAXIMUM DEFLECTION SHALL BE 22-1/2'. ANY ANGLE BENDS GREATER THAN 22-1/2' SHALL BE PREFABRICATED.

CONNECTOR PIPES SHALL CONNECT TO CATCH BASIN WALLS AT AN ANGLE NOT TO EXCEED 22-1/2° FROM PERPENDICULAR.

FACILITIES WHICH ARE NOT SPECIFICALLY LOCATED WITH ACTUAL VERTICAL AND HORIZONTAL CONTROLS, ARE LOCATED ONLY APPROXIMATELY AND TO THE BEST AVAILABLE INFORMATION PROVIDED BY VARIOUS OWNERS OF THE FACILITIES, AND SUPPLEMENTED BY VISUAL SURFACE INFORMATION WHERE APPROPRIATE. ACCURACY, LOCATION AND COMPLETENESS OF THIS INFORMATION SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO INITIATION OF CONSTRUCTION.

TWO WORKING DAYS BEFORE CONSTRUCTION, THE CONTRACTOR SHALL CONTACT APPROPRIATE UTILITIES TO FIND AND FLAG UNDERGROUND UTILITIES.

VERTICAL CONTROL IS BASED ON NATIONAL GEODETIC SURVEY.

UNLESS OTHERWISE NOTED, STATIONS SHOWN ON PIPE PROFILES ARE ALONG
CENTERLINE OF PIPE.

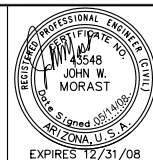
CATCH BASINS ARE STATIONED PERPENDICULAR TO THE CENTERLINE OF THE STREET AT THE CENTERLINE OF THE MAINTENANCE BASIN.


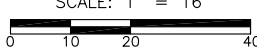

ALL EXISTING PRECAST CONCRETE SAFETY CURBS AND ALL EXISTING WOODEN PARKING CURBS, WHICH ARE INSIDE THE RIGHT OF WAY AND APPROXIMATELY PARALLEL TO THE NEW CURB LINE, SHALL BE RESET ON THE RIGHT OF WAY DIRECTLY OPPOSITE THEIR EXISTING LOCATION, WITH THE BACK EDGE ON THE RIGHT OF WAY LINE. ALL OTHER PRECAST CONCRETE SAFETY CURBS INSIDE THE STREET RIGHT OF WAY SHALL BE SALVAGED AND STOCK PILED FOR THE OWNER AT THE RIGHT OF WAY LINE.

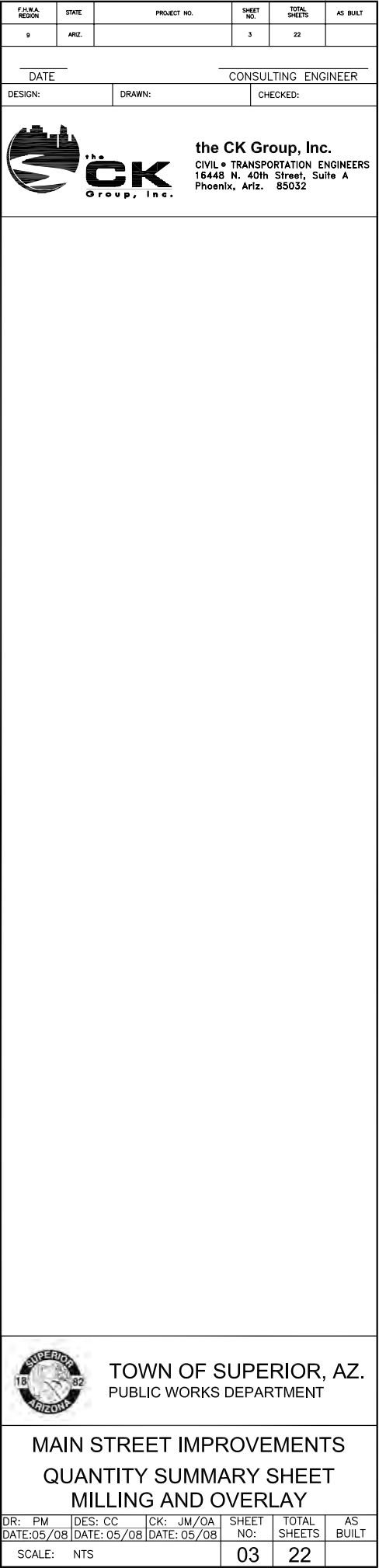
EXISTING PRECAST CONCRETE SAFETY CURBS OUTSIDE THE RIGHT OF WAY, WHICH ARE DISTURBED BY NEW CONSTRUCTION SHALL BE RESET IN THEIR ORIGINAL POSITION BY THE CONTRACTOR.

ALL EXISTING DRIVEWAYS AND ALL EXISTING ALLEYS SHALL BE GRADED TO MATCH THE NEW WORK. EXISTING SURFACING SHALL BE REMOVED AND REPLACED AS NECESSARY.

UNLESS OTHERWISE PROVIDED ON THE PLANS, EXISTING A.C. OR P.C.C. DRIVEWAYS
AND SIDEWALKS WHICH FALL WITHIN OR ABOUT THE RIGHT OF WAY SHALL BE
CONNECTED TO THE NEW WORK AS FOLLOWS:



F.H.M.A. REGION	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS	AS BUILT
9	ARIZ.		2	22	
DATE			CONSULTING ENGINEER		
DESIGN:		DRAWN:		CHECKED:	
<div><div>the CK Group, Inc. CIVIL • TRANSPORTATION ENGINEERS 16448 N. 40th Street, Suite A Phoenix, Ariz. 85032</div></div>					
<div><p>SCALE: 1" = 16'</p></div>					
<div><div>TOWN OF SUPERIOR, AZ. PUBLIC WORKS DEPARTMENT</div></div>					
MAIN STREET IMPROVEMENTS LEGEND AND GENERAL NOTES					
DR: PM DATE: 05/08	DES: CC DATE: 05/08	CK: JM/OA DATE: 05/08	SHEET NO:	TOTAL SHEETS	AS BUILT
SCALE: NTS			02	22	

[illegible][illegible]

REVISION BY TOWN OF SUPERIOR	
NO.	DESCRIPTION

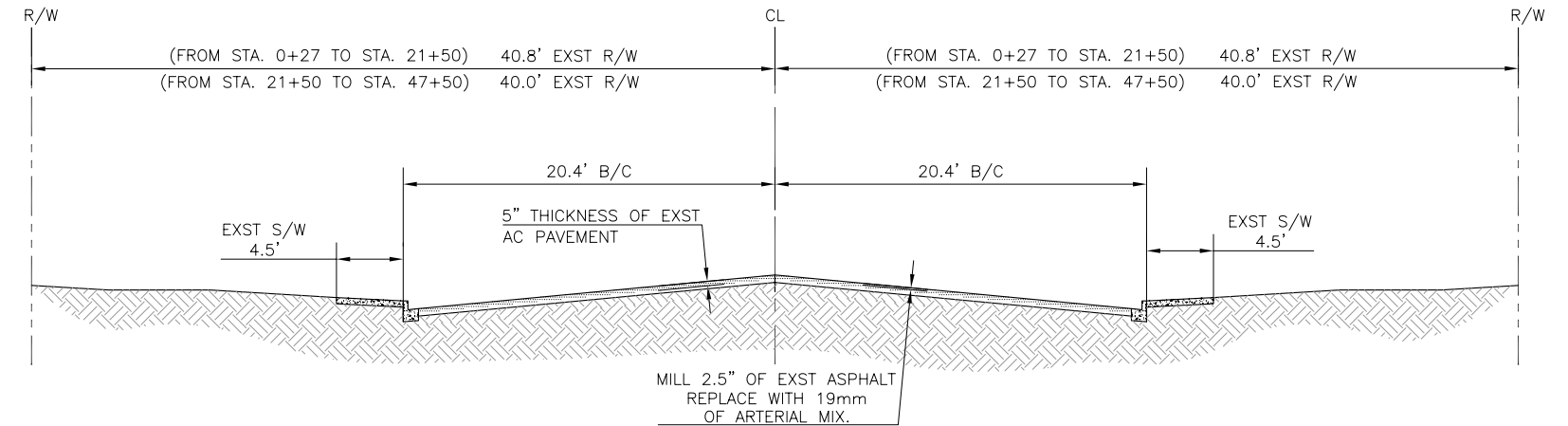
REVISION BY TOWN OF SUPERIOR	
NO.	DESCRIPTION

REVISION BY TOWN OF SUPERIOR	
NO.	DESCRIPTION

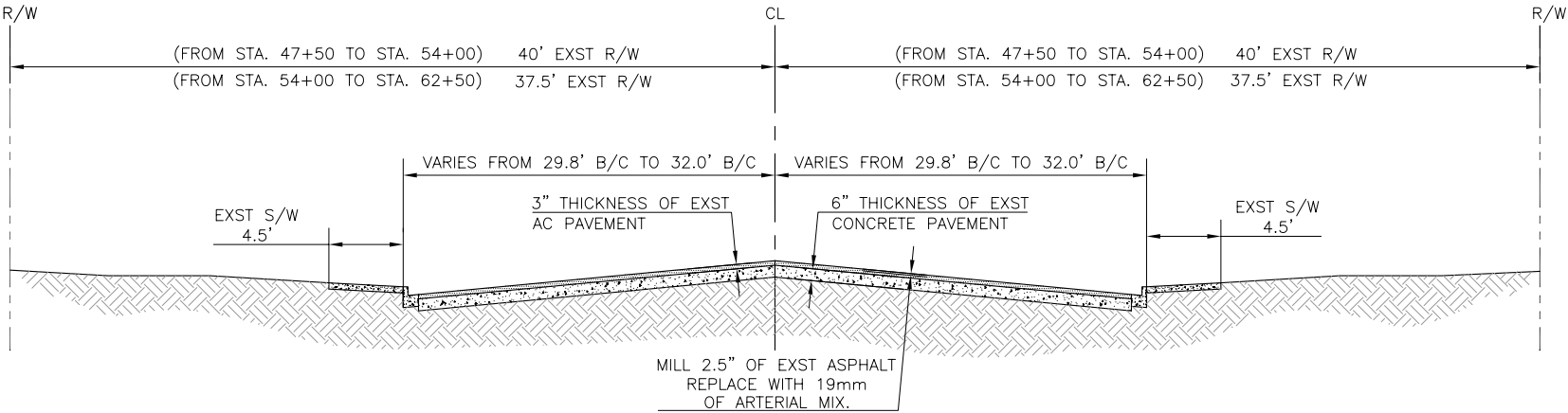
MAIN STREET IMPROVEMENTS
TOWN OF SUPERIOR, ARIZONA

FROM US60 HIGHWAY TO PINAL
COUNTY ADMINISTRATION OFFICE

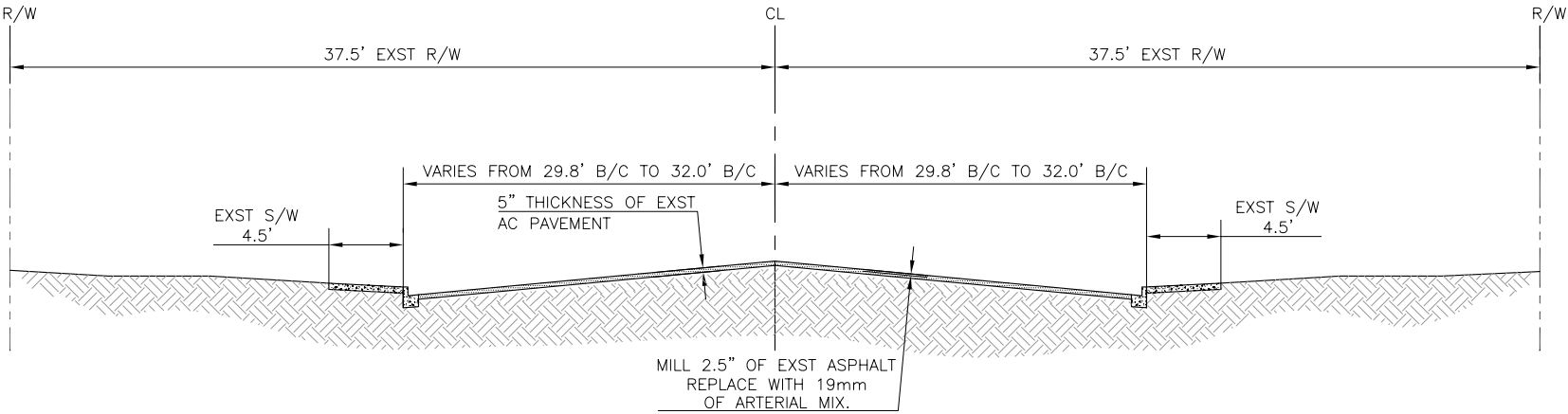
TYPICAL SECTION
STA. 00+27 TO STA. 47+50



TYPICAL SECTION
STA. 47+50 TO STA. 62+50



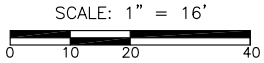
TYPICAL SECTION
STA. 62+50 TO STA. 66+94



FEDERAL REGION	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS	AS BUILT
9	ARZ.		4	22	
DATE			CONSULTING ENGINEER		
DESIGN:		DRAWN:		CHECKED:	



the CK Group, Inc.
CIVIL • TRANSPORTATION ENGINEERS
16448 N. 40th Street, Suite A
Phoenix, Ariz. 85032



TOWN OF SUPERIOR, AZ.
PUBLIC WORKS DEPARTMENT

MAIN STREET IMPROVEMENTS
TYPICAL SECTIONS

DR: PM	DES: CC	CK: JM/OA	SHEET NO:	TOTAL SHEETS	AS BUILT
DATE: 05/08	DATE: 05/08	DATE: 05/08			
SCALE: NTS			04	22	

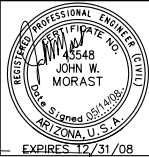
NO.	DESCRIPTION	REV BY	CKD BY	DATE

NO.	DESCRIPTION	REV BY	CKD BY	DATE

NO.	DESCRIPTION	REV BY	CKD BY	DATE

MAIN STREET IMPROVEMENTS
TOWN OF SUPERIOR, ARIZONA

FROM US60 HIGHWAY TO PINAL
COUNTY ADMINISTRATION OFFICE



FEDERAL REGION	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS	AS BUILT
9	ARZ.		5	22	

DATE		CONSULTING ENGINEER
DESIGN:	DRAWN:	CHECKED:

the CK Group, Inc.
CIVIL • TRANSPORTATION ENGINEERS
16448 N. 40th Street, Suite A
Phoenix, Ariz. 85032

NO.	REMOVALS	QTY.

NO.	NEW CONSTRUCTION	QTY.
1	MILL & OVERLAY (2.5")	5,136 SY
SM	SAWCUT	205 LF

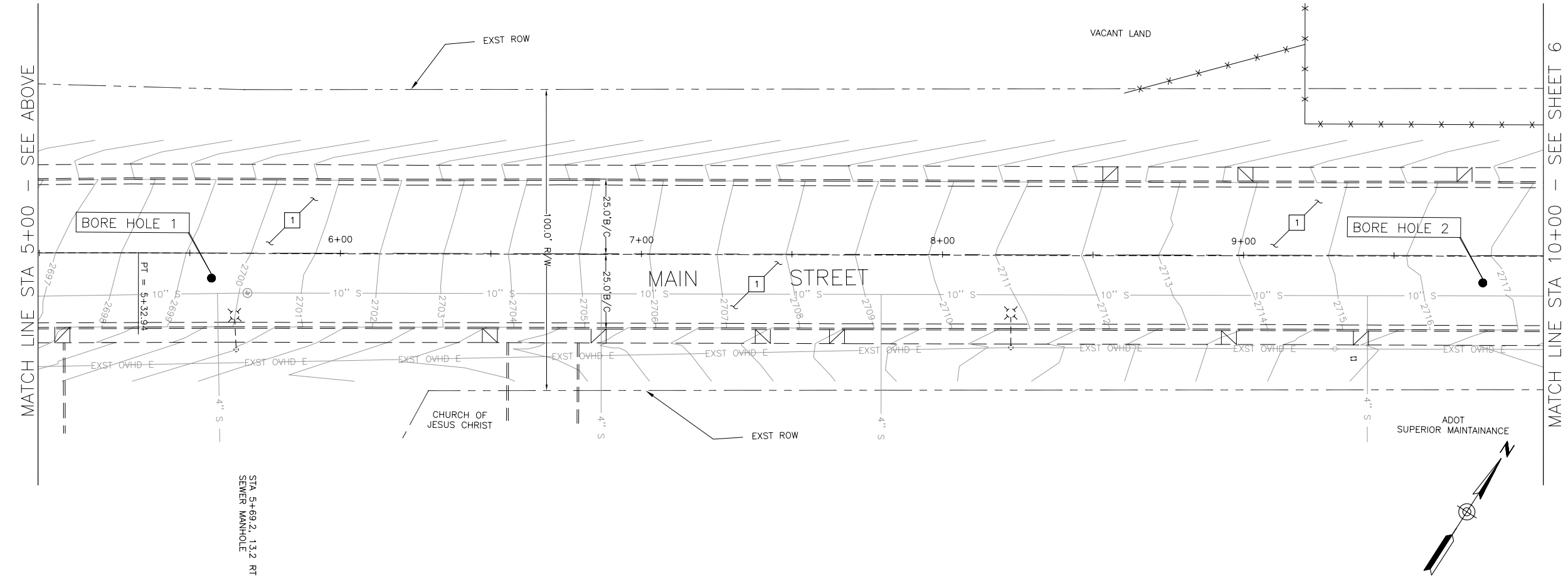
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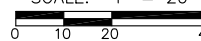

TOWN OF SUPERIOR, AZ.
PUBLIC WORKS DEPARTMENT

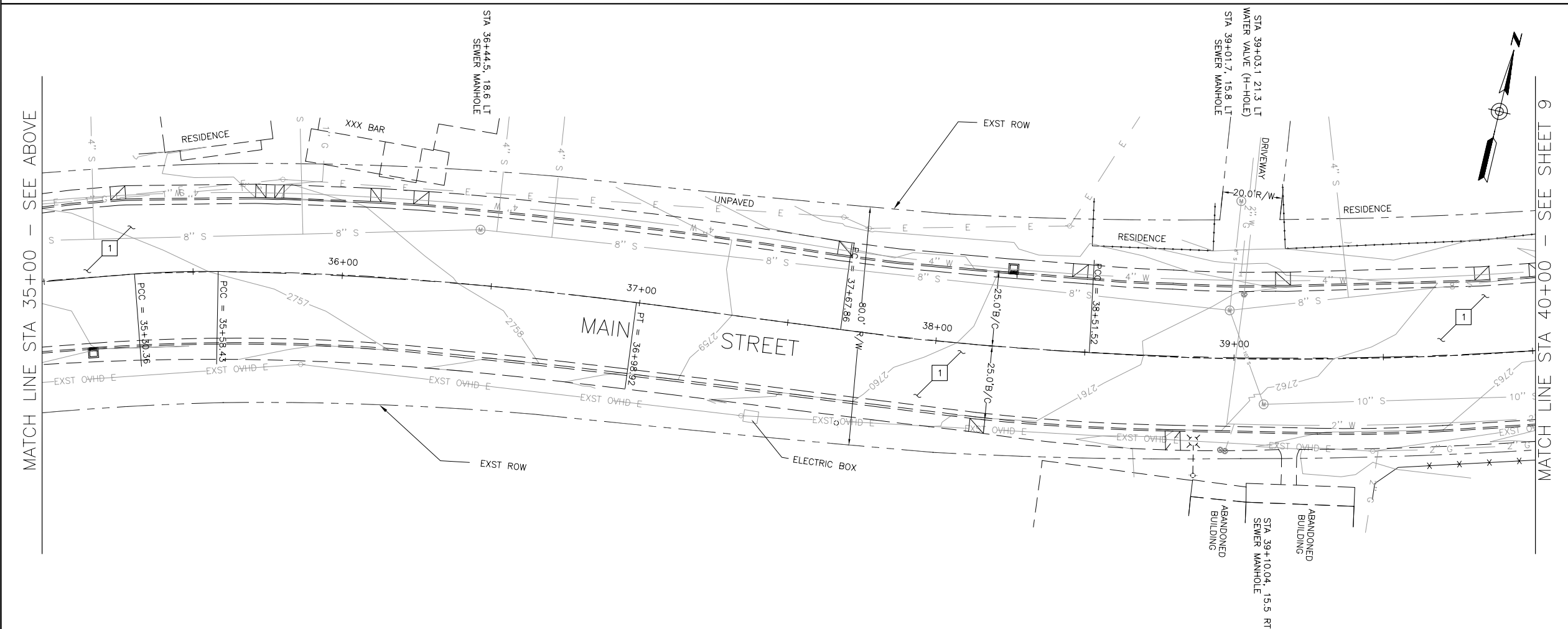
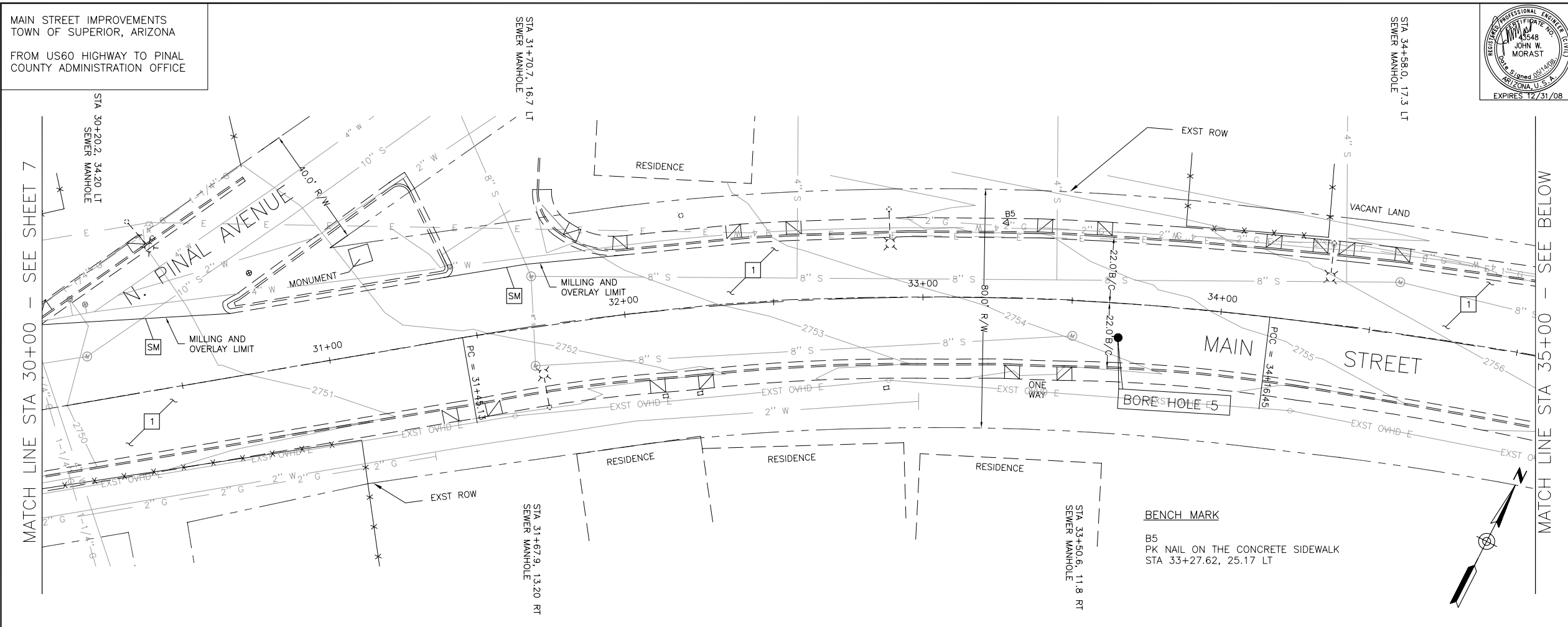
**MAIN STREET IMPROVEMENTS
PAVING PLAN
STA 0+00 - 10+00**



DR: PM	DES: CC	CK: AI	SHEET NO:	TOTAL SHEETS	AS BUILT
DATE: 05/08	DATE: 05/08	DATE: 05/08	5	22	

SCALE: 1" = 20'



F.H.W.A. REGION	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS	AS BUILT						
9	ARIZ.		8	22							
DATE _____CONSULTING ENGINEER _____											
DESIGN:		DRAWN:		CHECKED:							
<div><div>the CK Group, Inc. CIVIL • TRANSPORTATION ENGINEERS 16448 N. 40th Street, Suite A Phoenix, Ariz. 85032</div></div>											
NO.	REMOVALS			QTY.							
NO.	NEW CONSTRUCTION										
<div>1</div>	MILL & OVERLAY (2.5")			4,502 SY							
<div>SM</div>	SAWCUT			124 LF							
<div>SCALE: 1" = 20'</div> <div></div>											
<div><div>TOWN OF SUPERIOR, AZ. PUBLIC WORKS DEPARTMENT</div></div>											
<div>MAIN STREET IMPROVEMENTS PAVING PLAN STA 30+00 - 40+00</div>											
DR: PM	DES: CC	CK: AI	SHEET NO:	TOTAL SHEETS	AS BUILT						
DATE: 05/08	DATE: 05/08	DATE: 05/08	8	22							
SCALE: 1"= 20'											

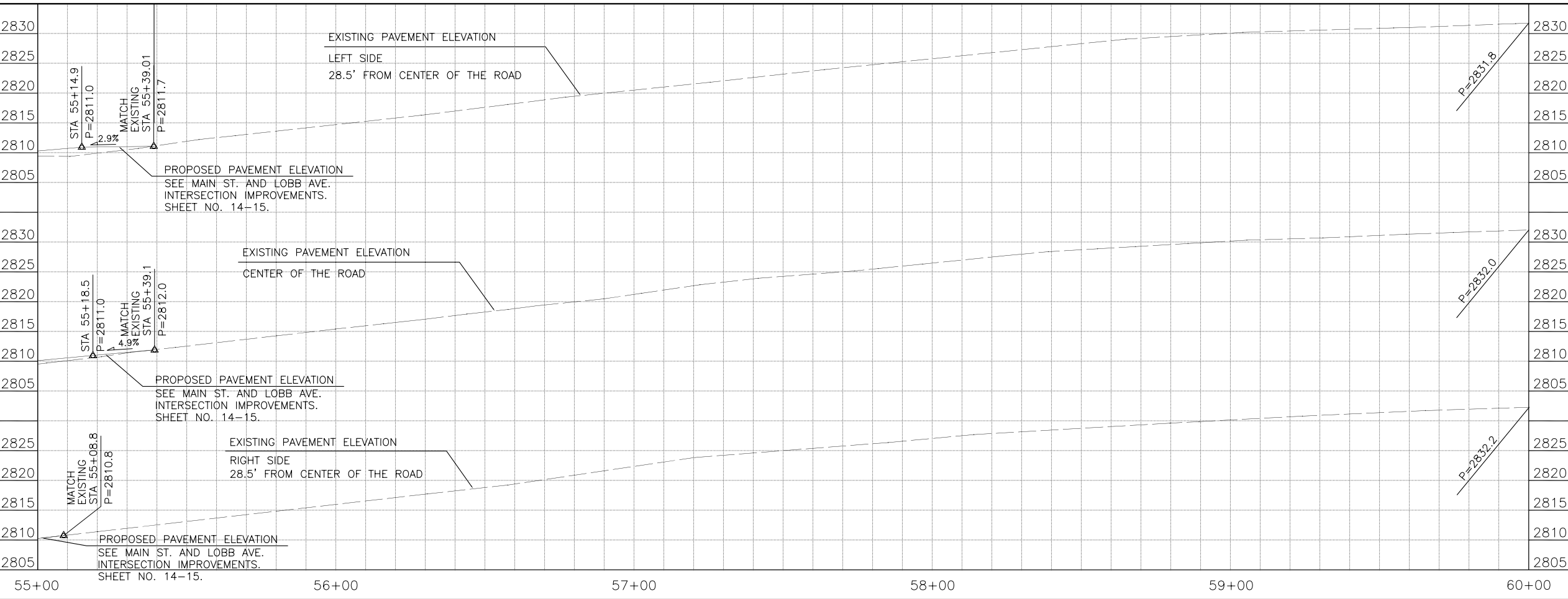
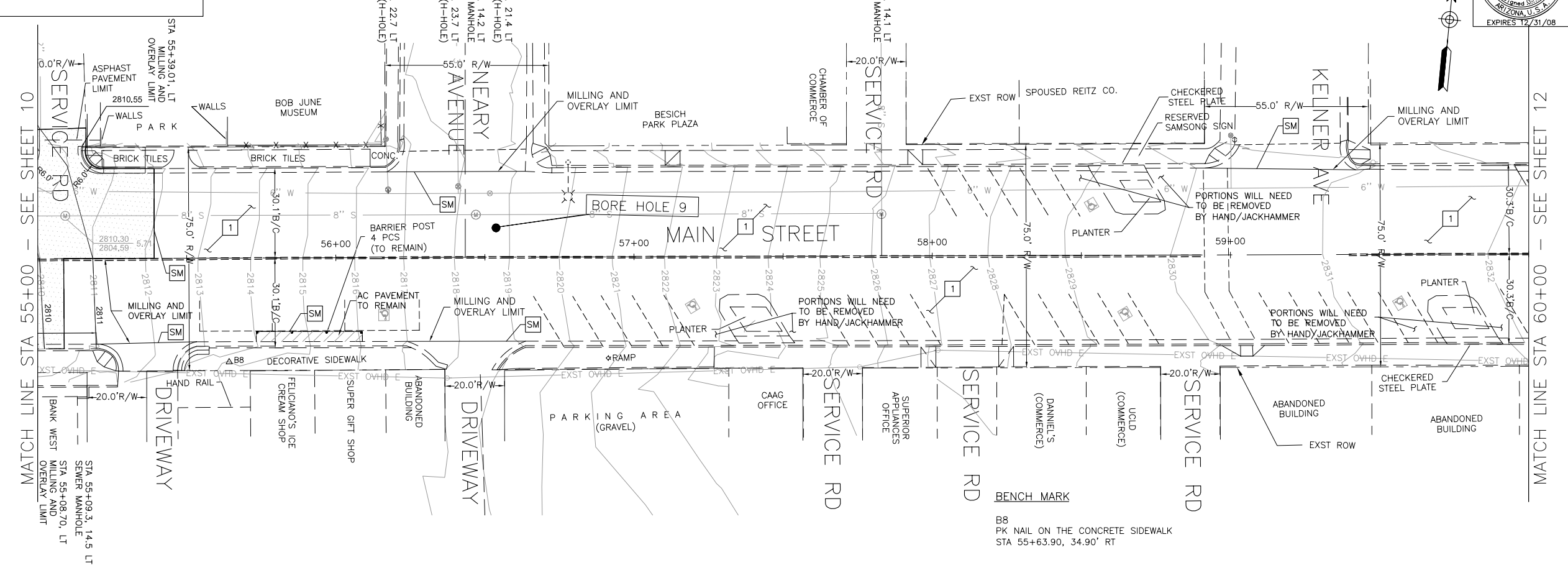




F.H.W.A. REGION	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS	AS BUILT
9	ARIZ.		9	22	
DATE		CONSULTING ENGINEER			
DESIGN:		DRAWN:		CHECKED:	
<div><div>the CK Group, Inc. CIVIL • TRANSPORTATION ENGINEERS 16448 N. 40th Street, Suite A Phoenix, Ariz. 85032</div></div>					
NO.	REMOVALS			QTY.	
NO.	NEW CONSTRUCTION				
<div>1</div>	MILL & OVERLAY (2.5")			5,932 SY	
<div>SM</div>	SAWCUT			185 LF	
<div><div>SCALE: 1" = 20'</div><div><div></div><div>0</div><div>10</div><div>20</div><div>40</div></div></div>					
<div><div></div><div>TOWN OF SUPERIOR, AZ. PUBLIC WORKS DEPARTMENT</div></div>					
MAIN STREET IMPROVEMENTS PAVING PLAN STA 40+00 - 50+00					
DR: PM	DES: CC	CK: AI	SHEET NO:	TOTAL SHEETS	AS BUILT
DATE: 05/08	DATE: 05/08	DATE: 05/08	9	22	
SCALE: 1" = 20'			9	22	

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MAIN STREET IMPROVEMENTS
TOWN OF SUPERIOR, ARIZONA

FROM US60 HIGHWAY TO PINAL
COUNTY ADMINISTRATION OFFICE



FHWA REGION	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS	AS BUILT
9	ARIZ.		11	22	
DATE _____ CONSULTING ENGINEER _____					
DESIGN:		DRAWN:	CHECKED:		
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NO.	REMOVALS			QTY.	
NO.	NEW CONSTRUCTION				
<div style="border: 1px solid black; width: 30px; height: 30px; display: flex; align-items: center; justify-content: center;">1</div>	MILL & OVERLAY (2.5")			3,068 SY	
<div style="border: 1px solid black; width: 30px; height: 30px; display: flex; align-items: center; justify-content: center;">SM</div>	SAWCUT			88 LF	
 <div style="display: inline-block; vertical-align: middle; margin-left: 20px;">TOWN OF SUPERIOR, AZ. PUBLIC WORKS DEPARTMENT</div>					
MAIN STREET IMPROVEMENTS PAVING PLAN STA 55+00 - 60+00					
DR: DATE: 05/08	DES: DATE: 05/08	CK: DATE: 05/08	SHEET NO:	TOTAL SHEETS	AS BUILT
SCALE: 1" = 20' HORIZONTAL 10' VERTICAL			11	22	



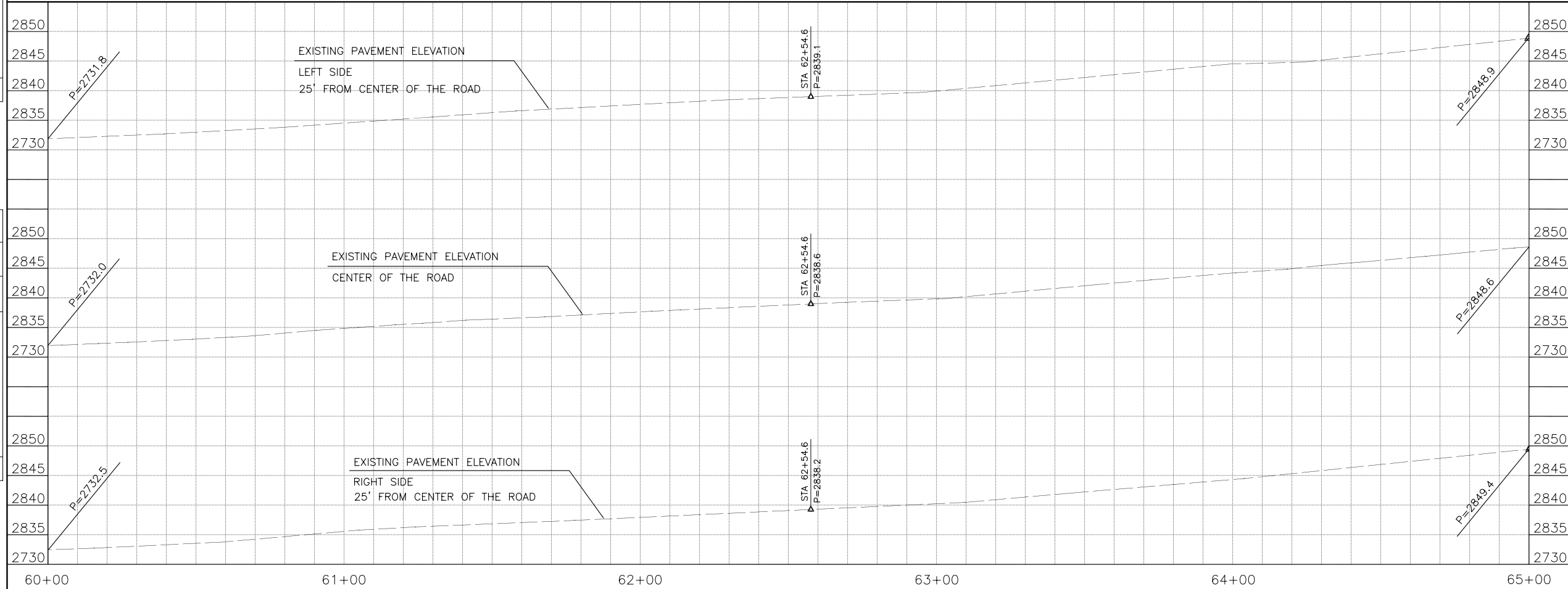
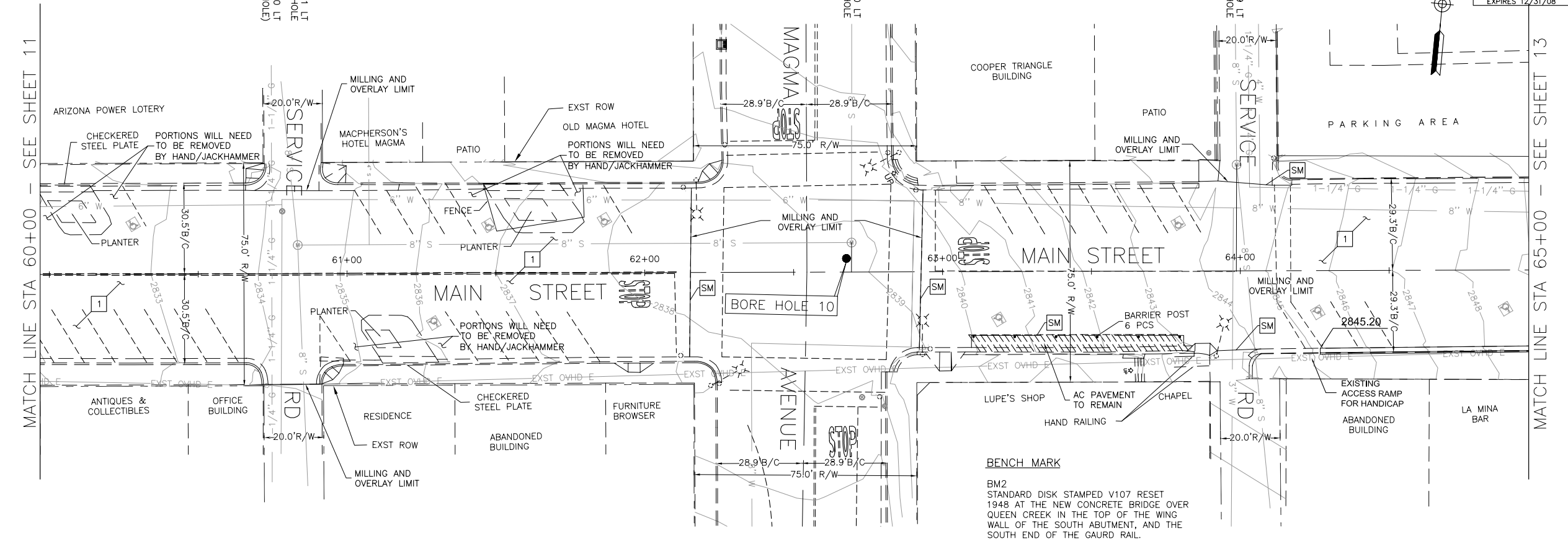
the CK Group, Inc.



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 16448 N. 40th Street, Suite A
 Phoenix, Ariz. 85032



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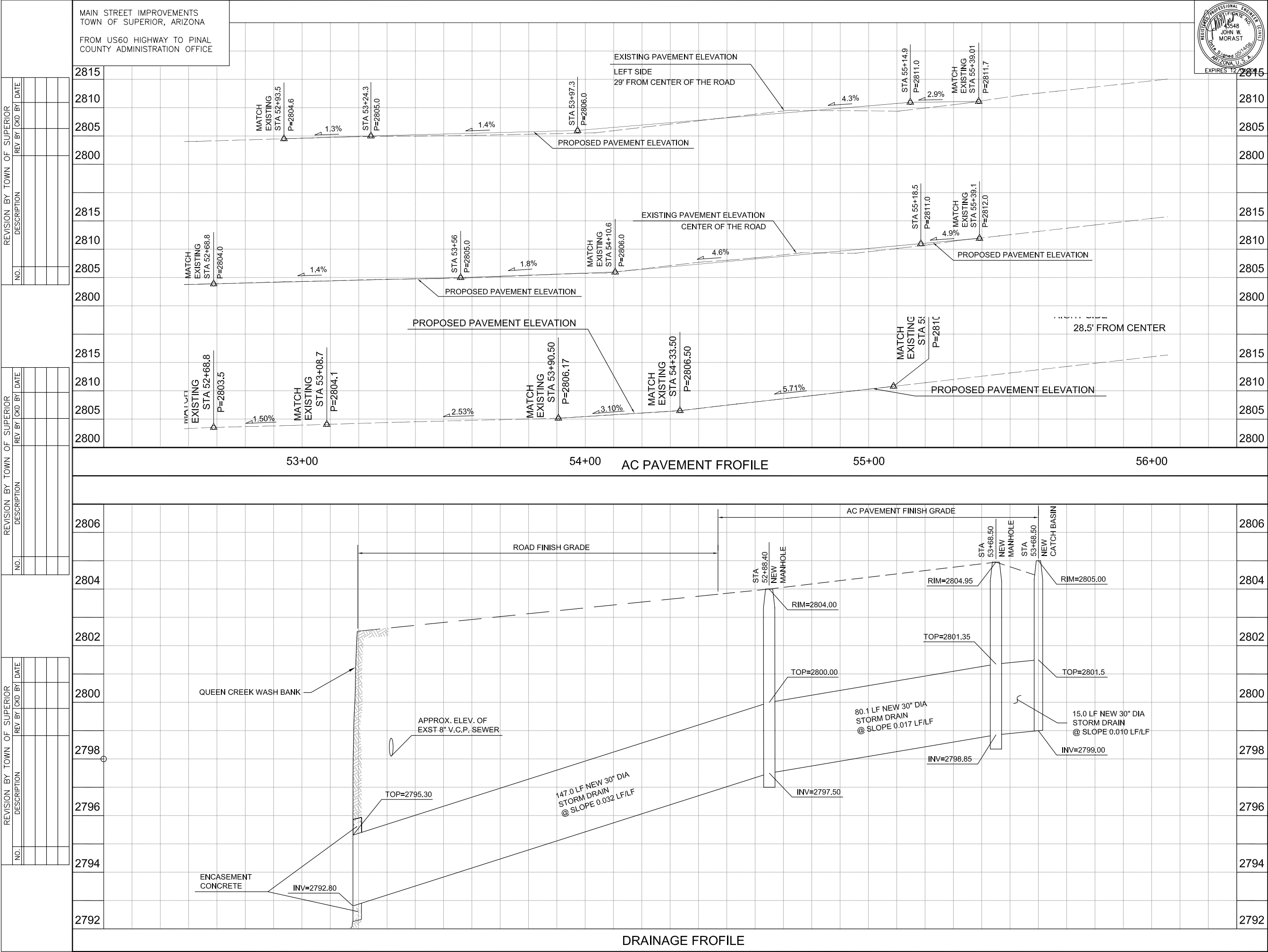
MAIN STREET IMPROVEMENTS
TOWN OF SUPERIOR, ARIZONA

FROM US60 HIGHWAY TO PINAL
COUNTY ADMINISTRATION OFFICE



FEDERAL REGION	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS	AS BUILT
9	ARIZ.		12	22	
DATE		CONSULTING ENGINEER			
DESIGN:		DRAWN:		CHECKED:	
<div><div>the CK Group, Inc. CIVIL • TRANSPORTATION ENGINEERS 16448 N. 40th Street, Suite A Phoenix, Ariz. 85032</div></div>					
NO.	REMOVALS			QTY.	
NO.	NEW CONSTRUCTION				
<input type="checkbox"/>	MILL & OVERLAY (2.5")			2,995	SY
<input type="checkbox"/>	SAWCUT			224	LF
<div><div>TOWN OF SUPERIOR, AZ. PUBLIC WORKS DEPARTMENT</div></div>					
MAIN STREET IMPROVEMENTS PAVING PLAN STA 60+00 - 65+00					
DR: DATE: 05/08	DES: DATE: 05/08	CK: DATE: 05/08	SHEET NO: 12	TOTAL SHEETS 22	AS BUILT
SCALE: 1" = 20' HORIZONTAL 10' VERTICAL					

F.H.W.A. REGION	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS	AS BUILT
9	ARIZ.		14	22	
DATE _____ CONSULTING ENGINEER _____					
DESIGN:		DRAWN:		CHECKED:	
<div><div>the CK Group, Inc. CIVIL & TRANSPORTATION ENGINEERS 16448 N. 40th Street, Suite A Phoenix, Ariz. 85032</div></div>					
NO.	REMOVALS			QTY.	
②	CURB & GUTTER			281 LF	
③	CONCRETE SIDEWALK			1030 SF	
④	CONCRETE DRIVEWAY			136 SF	
⑤	DECORATIVE SIDEWALK			122 SF	
NO.	NEW CONSTRUCTION				
1	MILLING AND OVERLAY			1,780 SY	
2	CONC. CURB & GUTTER, MAG STD. DET. 220, TYPE 'A', H=6".			215 LF	
3	CONCRETE SIDEWALK, MAG STD. DET. 230, 6" WIDE			919 SF	
4	DECORATIVE SIDEWALK, MATCH EXISTING			122 SF	
5	CONCRETE DRIVEWAY			136 SF	
6	CONCRETE SIDEWALK RAMP MAG STD. 231, TYPE A			119 SF	
7	ADJUST SEWER MANHOLE COVER ELEVATION			2 EA	
8	ADJUST WATER VALVE COVER ELEVATION			2 EA	
9	HANDICAP RAMP			133 SF	
SM	SAWCUT			231 LF	
NO.	NEW STORM SEWER				
1	CONCRETE CATCH BASIN, M-2 (L=17')			1 EA	
2	48" STORM DRAIN MANHOLE			2 EA	
3	30" RCP STORM DRAIN PIPE			242 LF	
NOTE: CONTRACTOR WILL BE REQUIRED TO MANUALLY COMPACT AS NEEDED ON BARRIER POSTS LOCATION. SEE SHEETS COMPACT AS NEEDED ON BARRIER POSTS LOCATION.					
<div><div>TOWN OF SUPERIOR, AZ. PUBLIC WORKS DEPARTMENT</div></div>					
MAIN STREET IMPROVEMENTS INTERSECTION IMPROVEMENTS MAIN STREET AND LOBB AVE. INT.					
DR: DATE: 05/08	DES: DATE: 05/08	CK: DATE: 05/08	SHEET NO:	TOTAL SHEETS	AS BUILT
SCALE: 1"=16' HORIZONTAL			14	22	



FALWA REGION	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS	AS BUILT
9	ARZ.		15	22	

DATE: / / CONSULTING ENGINEER

DESIGN: DRAWN: CHECKED:

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16448 N. 40th Street, Suite A
Phoenix, Ariz. 85032

TOWN OF SUPERIOR, AZ.
PUBLIC WORKS DEPARTMENT

**MAIN STREET IMPROVEMENTS
INTERSECTION IMPROVEMENTS
MAIN STREET AND LOBB AVE. INT.**

DR: PM	DES: CC	CK: AI/OA	SHEET NO:	TOTAL SHEETS	AS BUILT
DATE: 05/08	DATE: 05/08	DATE: 05/08	15	22	

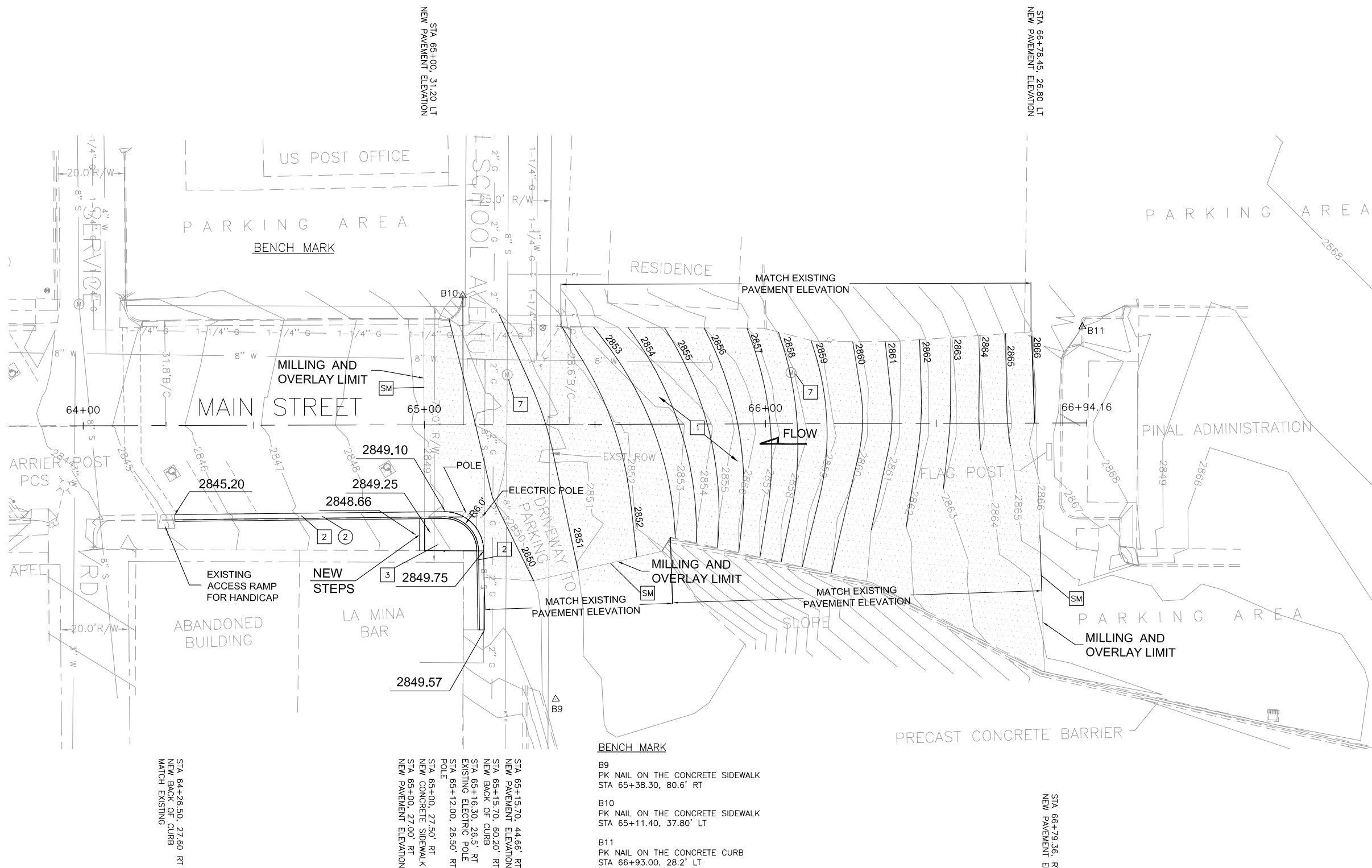
SCALE: 1"=16' HORIZONTAL
1"=8' VERTICAL

REVISION BY TOWN OF SUPERIOR				
NO.	DESCRIPTION	REV BY	CHKD BY	DATE

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MAIN STREET IMPROVEMENTS
TOWN OF SUPERIOR, ARIZONA

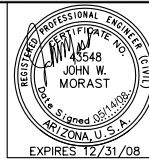
FROM US60 HIGHWAY TO PINAL
COUNTY ADMINISTRATION OFFICE



LEGEND

- NEW CURB & GUTTER
- EXISTING CURB & GUTTER
- EXISTING CONTOURS
- PROPOSED CONTOURS
- PROPOSED ELEVATION

QUANTITIES	
ASPHALT BACKFILL	40.71 CY



FLA/RJIA REGION	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS	AS BUILT
9	ARIZ.		16	22	

DATE		CONSULTING ENGINEER	
DESIGN:	DRAWN:	CHECKED:	



the CK Group, Inc.

CIVIL • TRANSPORTATION ENGINEERS

16448 N. 19th Street, Suite A
Phoenix, Ariz. 85032

NO.	REMOVALS	QTY.
(2)	CURB & CUTTER	78.9 LF

NO.	NEW CONSTRUCTION	
1	MILLING AND OVERLAY	1987.5 SY
2	CONC. CURB & GUTTER, MAG STD. DET. 220, TYPE 'A', H=6".	118 LF
3	CONCRETE SIDEWALK, MAG STD. DET. 230, 6" WIDE	51.1 SF
7	ADJUST SEWER MANHOLE	2 EA
SM	SAWCUT	237.95 LF



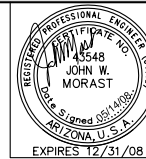
TOWN OF SUPERIOR, AZ.
PUBLIC WORKS DEPARTMENT

MAIN STREET IMPROVEMENTS INTERSECTION IMPROVEMENTS 300' FROM MAGMA AVE & MAIN ST

DR:	DES:	CK:	SHEET	TOTAL	AS
DATE:05/08	DATE: 05/08	DATE: 05/08	NO:	SHEETS	BUILT
SCALE: 1"=16' HORIZONTAL			16	22	

MAIN STREET IMPROVEMENTS
TOWN OF SUPERIOR, ARIZONA

FROM US60 HIGHWAY TO PINAL
COUNTY ADMINISTRATION OFFICE



F.H.W.A. REGION	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS	AS BUILT
9	ARIZ.		17	22	


DATE

CONSULTING ENGINEER

DESIGN:

DRAWN:

CHECKED:

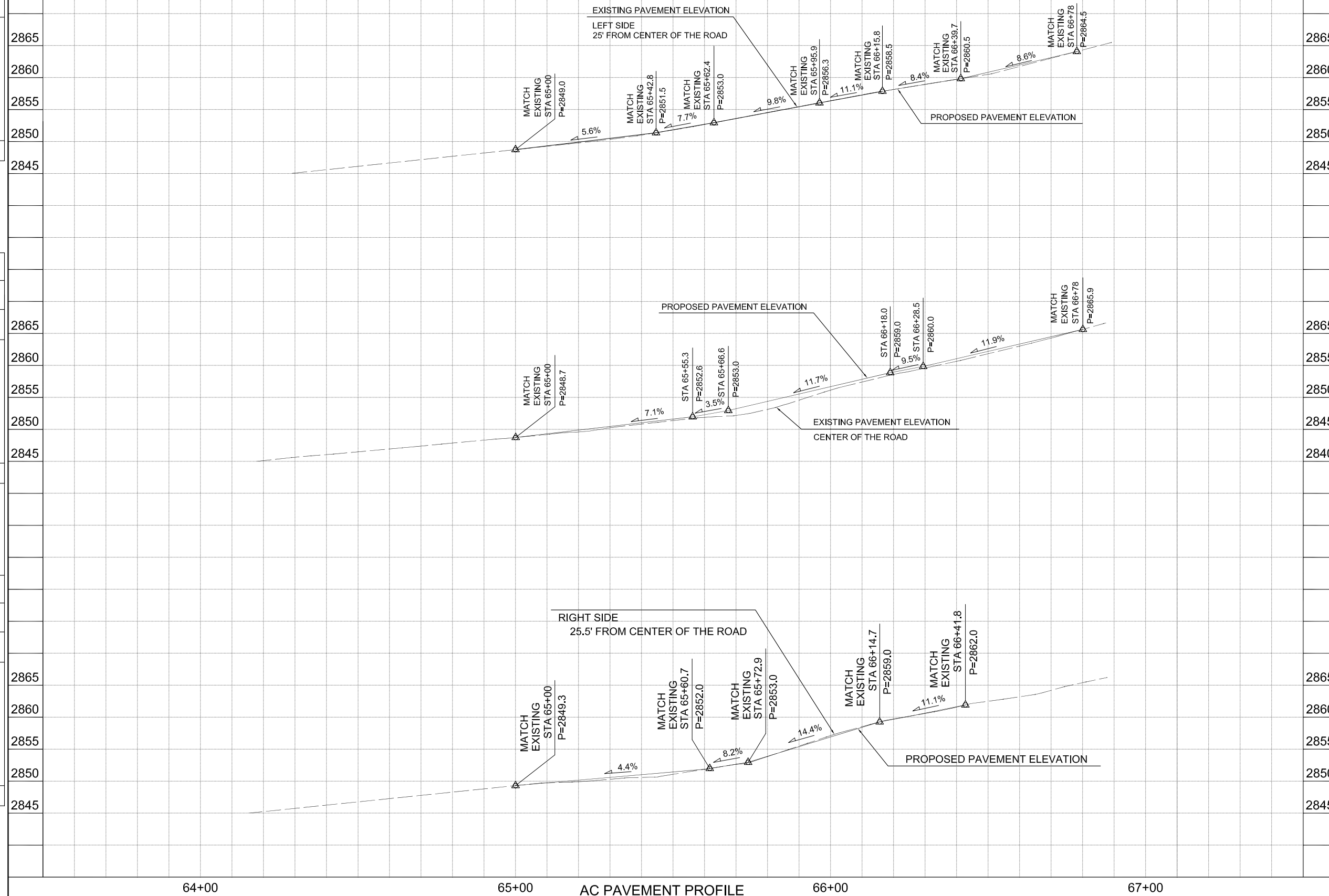


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16448 N. 40th Street, Suite A

Phoenix, Ariz. 85032



TOWN OF SUPERIOR, AZ.
PUBLIC WORKS DEPARTMENT

<p>MAIN STREET IMPROVEMENTS</p> <p>INTERSECTION IMPROVEMENTS</p> <p>300' FROM MAGMA AVE & MAIN ST</p>

DR: PM	DES: CC	CK: AI/OA	SHEET NO:	TOTAL SHEETS	AS BUILT
DATE: 05/08	DATE: 05/08	DATE: 05/08			
SCALE: 1"=16' HORIZONTAL			17	22	

REVISION BY TOWN OF SUPERIOR

NO.

DESCRIPTION

REV BY

CHK BY

DATE

REVISION BY TOWN OF SUPERIOR

NO.

DESCRIPTION

REV BY

CHK BY

DATE

REVISION BY TOWN OF SUPERIOR

NO.

DESCRIPTION

REV BY

CHK BY

DATE

MAIN STREET IMPROVEMENTS
TOWN OF SUPERIOR, ARIZONA

FROM US60 HIGHWAY TO PINAL
COUNTY ADMINISTRATION OFFICE

PAVEMENT MARKING CONSTRUCTION NOTES

1. ALL PAVEMENT MARKINGS SHALL CONFORM TO THE ARIZONA DEPARTMENT OF TRANSPORTATION AND SPECIFICATIONS UNLESS OTHERWISE SPECIFIED IN THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, (LATEST EDITIONS), OR AS NOTED HEREIN.

2. ANY QUESTION CONCERNING PAVEMENT MARKING SHALL BE SUBMITTED TO THE PINAL COUNTY TRAFFIC ENGINEERING SECTION.

3. INSTALLATION OF PAVEMENT MARKINGS SHALL BE LOCATED TO ENSURE THAT THE FINAL SURFACE COURSE IS PLACED SO THAT THE STRIPING IS OFFSET 1 FOOT CLEAR OF THE CONSTRUCTION JOINT, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

4. THE DIMENSIONS SHOWN TO PAVEMENT STRIPING ARE TO THE CENTER OF THE STRIPING OR, IN THE CASE OF DOUBLE STRIPING, TO THE CENTER OF THE DOUBLE STRIPING.

5. THE FINAL STRIPING SHALL BE 60 MIL (0.060 INCH) THICK HOT-SPRAYED THERMOPLASTIC REFLECTORIZED STRIPING.

6. THE PAVEMENT ARROW, SYMBOLS AND WORD LEGEND SHALL BE WHITE 90 MIL (0.090 INCH) THICK ALKYD EXTRUDED THERMOPLASTIC REFLECTORIZED MARKINGS.

7. THE ROADWAY SURFACE SHALL BE CLEANED TO THE SATISFACTION OF THE ENGINEER, BY SWEEPING AND AIR-JET BLOWING, IMMEDIATELY PRIOR TO THE PLACEMENT OF ALL PAVEMENT MARKINGS. THE ROADWAY SURFACE SHALL BE DRY AND THE AIR AND PAVEMENT TEMPERATURES SHALL NOT BE LESS THAN 50 DEGREES F FOR THE PLACEMENT OF THERMOPLASTIC MARKINGS.

8. ALL RAISED PAVEMENT MARKERS (OPTIONAL) SHALL HAVE AN ABRASION RESISTANT COATING ON THE FACE OF THE PRISMATIC REFLECTORS AND SHALL CONFORM TO THE ADOT STANDARD DRAWING M-19. THEY SHALL BE INSTALLED WITH A BITUMINOUS ADHESIVE WHICH IS ON THE ADOT APPROVED PRODUCTS LIST.

9. WHERE RAISED PAVEMENT MARKERS ARE PLACED ALONG SOLID STRIPING, THE NEAREST EDGE OF EACH MARKER SHALL BE OFFSET 2 INCHES FROM THE NEAREST EDGE OF THE STRIPING.

10. ALL SIGNS SHALL BE IN COMPLIANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), THE ADOT SIGNING AND MARKING STANDARD DRAWINGS, AND THE TRAFFIC ENGINEERING MANUAL OF APPROVED SIGNS.

11. THE BOTTOM OF EACH SIGN SHALL BE AT LEAST 7 FEET ABOVE THE NEAREST EDGE OF PAVEMENT AND AT LEAST 7 FEET ABOVE THE GROUND UNDER THE SIGN. SIGN POSTS SHALL BE SQUARE TUBING AND SHALL HAVE SLIP BASE FOUNDATION PER ADOT STANDARD DRAWING S- 1.

12. SIGNS SHALL BE LOCATED OR RELOCATED SO THE NEAREST EDGE OR CORNER OF EACH SIGN IS OFFSET 2 FEET BEHIND THE BLOCK OF THE SIDEWALK OR 6 FEET MINIMUM BEHIND THE EDGE OF PAVEMENT WHERE NO SIDEWALK EXISTS.

13. ALL SIGNS SHALL BE FABRICATED OF FLAT SHEET ALUMINUM WITH DIRECT APPLIED COPY OR SILK-SCREENED LEGEND. RETROREFLECTIVE SHEETING SHALL BE IN ACCORDANCE WITH ADOT SECTION 1007.

14. TURN BAY "GAP" SHALL BE AS FOLLOWS:

60' FOR 25-35 MPH

90' FOR 40-50 MPH

140' FOR 55-65 MPH

15. TURN BAY STORAGE SHALL BE A MINIMUM OF 100' UNLESS DIRECTED OTHERWISE BY THE PINAL COUNTY TRAFFIC ENGINEERING SECTION.

16. PAVEMENT LEGEND, WORDS AND ARROWS SHALL LOCATED IN ACCORDANCE WITH ADOT STANDARD DRAWING M-11.


17. DISTANCE AS DETERMINED BY THE PINAL COUNTY TRAFFIC ENGINEERING SECTION: BASED ON REQUIRED SIGHT DISTANCE, ROADWAY GEOMETRY, ACCIDENT HISTORY, JUDGMENT, POSTED AND ACTUAL SPEED, ETC.

REGISTERED PROFESSIONAL ENGINEER
NO. 43548
JOHN W. MORAST
PINAL COUNTY
ARIZONA, U.S.A.
EXPIRES 12/31/08

FALWA REGION	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS	AS BUILT
9	ARZ.		18	22	

DATECONSULTING ENGINEER


DESIGN:DRAWN:CHECKED:



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16448 N. 40th Street, Suite A
Phoenix, Ariz. 85032

SIGNING/PAVEMENT MARKING QUANTITIES

DESCRIPTION	UNIT	QUANTITY
4" WHITE THERMOPLASTIC TRAFFIC STRIPE	LF	3,950
6" WHITE THERMOPLASTIC TRAFFIC STRIPE	LF	18,950
12" WHITE THERMOPLASTIC TRAFFIC STRIPE	LF	2,050
18" WHITE THERMOPLASTIC TRAFFIC STRIPE	LF	60
6" YELLOW THERMOPLASTIC TRAFFIC STRIPE	LF	12,440
THERMOPLASTIC PAVEMENT LEGEND BIKE SIGN	EA	14
THERMOPLASTIC PAVEMENT LEGEND HANDICAP SIGN	EA	21
SIGN PANEL	SF	120
TELESPAR POST	LF	252
SIGN FOUNDATION	EA	20
REMOVE & SALVAGE SIGN	EA	2



TOWN OF SUPERIOR, AZ.
PUBLIC WORKS DEPARTMENT

MAIN STREET IMPROVEMENTS

LEGEND AND GENERAL NOTES

DR: PP	DES: PP	CK: AI	SHEET NO:	TOTAL SHEETS	AS BUILT
DATE: 05/08	DATE: 05/08	DATE: 05/08			
SCALE: NTS			18	22	

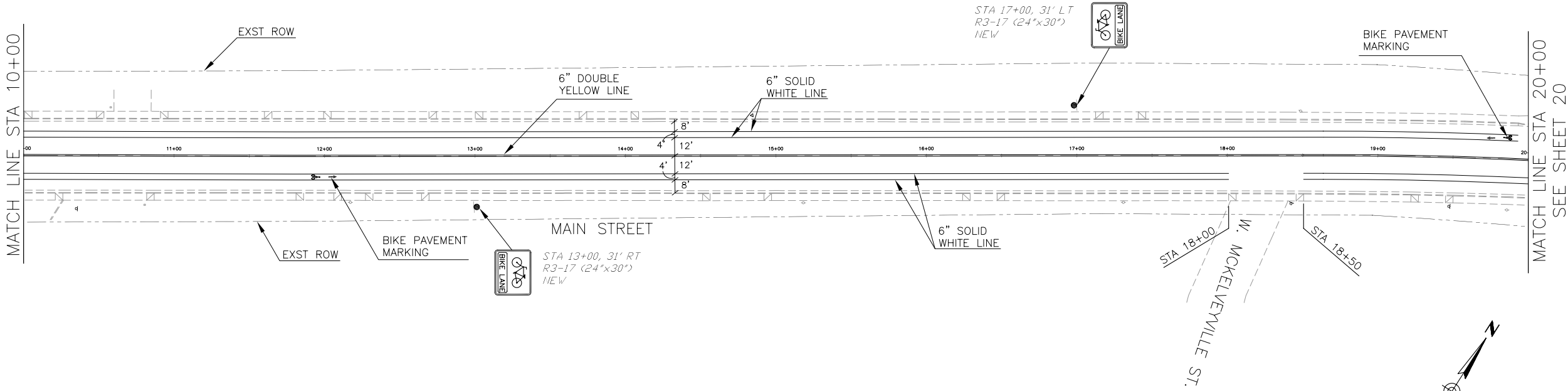
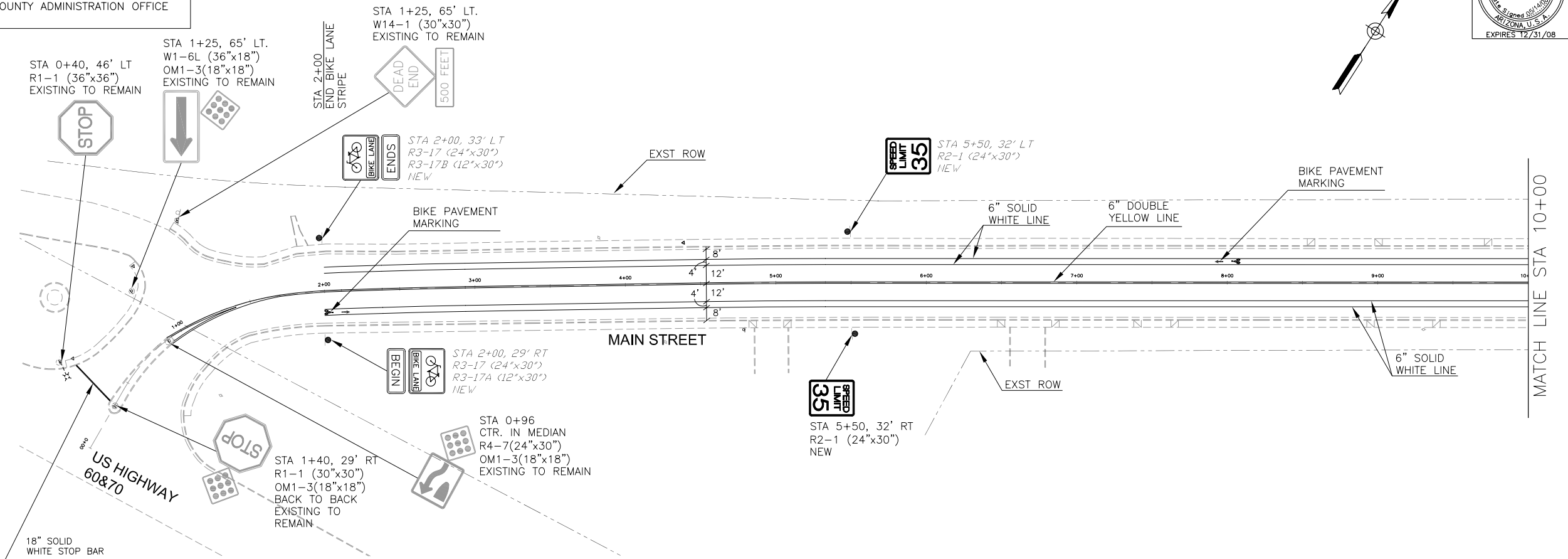
REVISION BY TOWN OF SUPERIOR	
NO.	DESCRIPTION

REVISION BY TOWN OF SUPERIOR	
NO.	DESCRIPTION

REVISION BY TOWN OF SUPERIOR	
NO.	DESCRIPTION

MAIN STREET IMPROVEMENTS
TOWN OF SUPERIOR, ARIZONA

FROM US60 HIGHWAY TO PINAL
COUNTY ADMINISTRATION OFFICE



FALWA REGION	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS	AS BUILT
9	ARZ.		19	22	

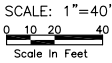
DATE: _____ CONSULTING ENGINEER: _____

DESIGN: _____ DRAWN: _____ CHECKED: _____

the CK Group, Inc.
CIVIL • TRANSPORTATION ENGINEERS
16448 N. 40th Street, Suite A
Phoenix, Ariz. 85032

NOTES

1. SEE BIKE LANE PAVEMENT MARKING DETAIL B ON SHEET 18.

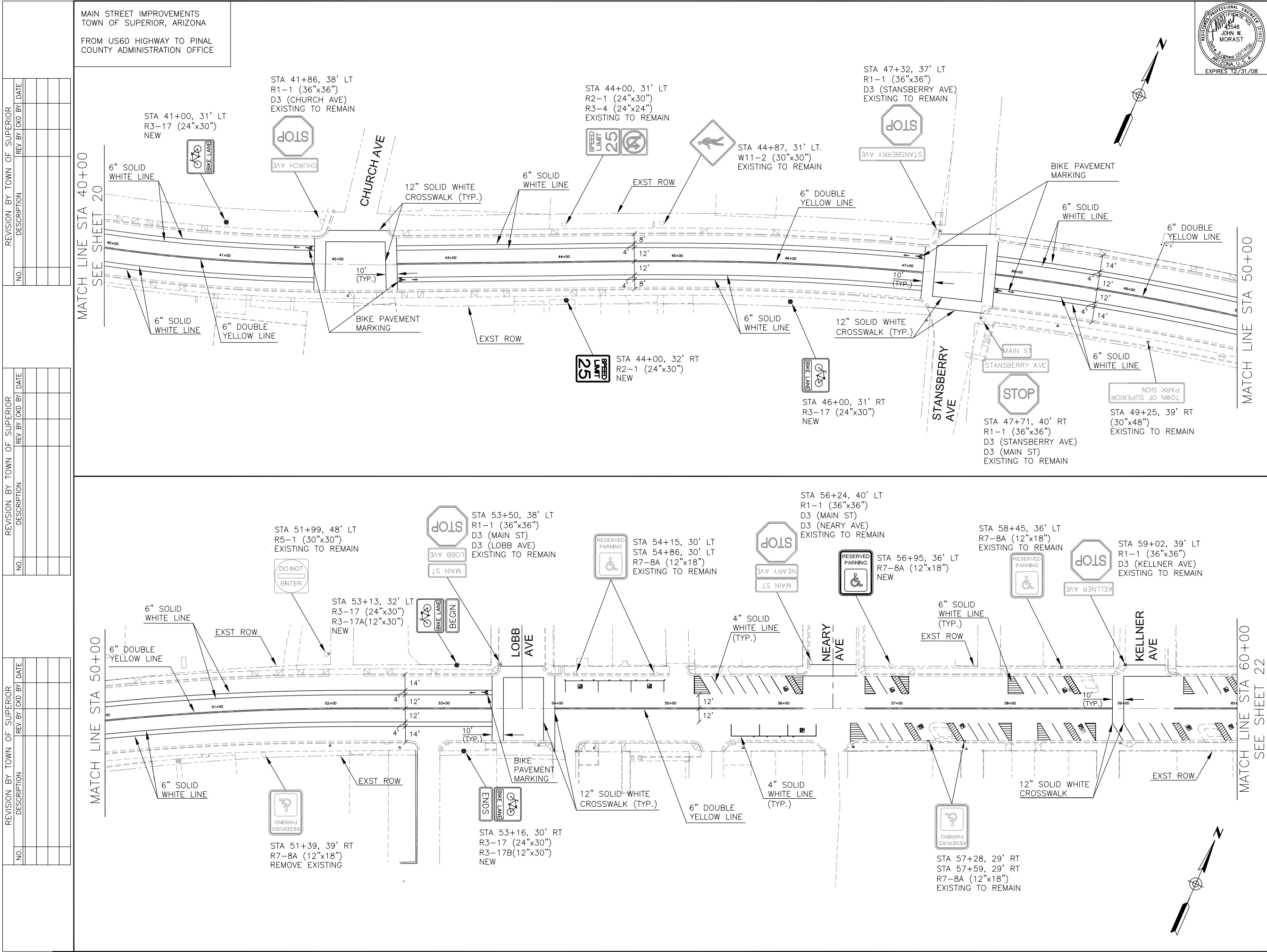


TOWN OF SUPERIOR, AZ.
PUBLIC WORKS DEPARTMENT

MAIN STREET IMPROVEMENTS
SIGNING & MARKING PLAN
STA 0+00 - 20+00

DR: PP	DES: PP	CK: AI	SHEET NO:	TOTAL SHEETS	AS BUILT
DATE: 05/08	DATE: 05/08	DATE: 05/08	19	22	

SCALE: 1"= 40'



FALWA REGION	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS	AS BUILT
9	ARZ.		21	22	

DATE		CONSULTING ENGINEER
DESIGN:	DRAWN:	CHECKED:

the CK Group, Inc.
CIVIL • TRANSPORTATION ENGINEERS
16448 N. 40th Street, Suite A
Phoenix, Ariz. 85032

NOTES

- SEE BIKE LANE PAVEMENT MARKING B ON SHEET 18.
- SEE HANDICAP SYMBOL & PARKING SPACE DETAIL A & C ON SHEET 18.
- ALL HANDICAP PARKING SPACES SHALL BE STRIPED WITH HANDICAP PARKING SYMBOL PER DETAIL A. INSTALL R7-8A(12"x18") SIGN PER DETAIL D.

SCALE: 1"=40'

TOWN OF SUPERIOR, AZ.
PUBLIC WORKS DEPARTMENT

MAIN STREET IMPROVEMENTS
SIGNING & MARKING PLAN
STA 40+00 - 60+00

DR: PP	DES: PP	CK: AI	SHEET NO:	TOTAL SHEETS	AS BUILT
DATE: 05/08	DATE: 05/08	DATE: 05/08	21	22	

SCALE: 1"= 40'

GLOBE APPLICATION



SURFACE TRANSPORTATION BLOCK GRANT PROGRAM (STBGP) APPLICATION

GENERAL PROJECT INFORMATION					
SPONSORING AGENCY:	City of Globe	DATE SUBMITTED:	11/16/2023		
CONTACT NAME:	Luis Chavez	TITLE:	City Engineer		
EMAIL ADDRESS:	lochavez@globeaz.gov	PHONE #:	928-961-1748		
<input type="checkbox"/> ROADWAY IMPROVEMENT	Roadway Name:				
	Starting Location:				
	Ending Location:				
	Length (to the 0.1 of a mile):				
	# of Lanes (Before & After):	Before:		After:	
<input type="checkbox"/> INTERSECTION IMPROVEMENT	Roadway Name "A":				
	Roadway Name "B":				
<input type="checkbox"/> BRIDGE IMPROVEMENT	<input type="checkbox"/> Restoration/Operational <input type="checkbox"/> Replacement <input type="checkbox"/> Widening	Bridge Sufficiency Rating (LINK to ADOT NBI Table)			
		Structurally Deficient?	<input type="checkbox"/> Yes	<input type="checkbox"/> No	
		Functionally Obsolete?	<input type="checkbox"/> Yes	<input type="checkbox"/> No	
<input checked="" type="checkbox"/> OTHER	Description of project type:	Detached Pedestrian Pathway			
FEDERAL FUNCTIONAL CLASSIFICATION (LINK: FEDERAL FUNCTIONAL CLASSIFICATION MAPS):		Minor Collector			
AVERAGE ANNUAL DAILY TRAFFIC (AADT) COUNT: (LINK: AADT COUNTS):		4894	DATE OF AADT COUNT:		2023

COST ESTIMATE & PROJECT PROGRAMMING

<input checked="" type="checkbox"/> DESIGN	FY Program Year:		
	Funding Source Request:	<input type="checkbox"/> STBGP	<input type="checkbox"/> HURF Exchange
	Other Non-Local Funding Sources to be Utilized:	<input type="checkbox"/> _____	
	Total Cost Estimate:	\$72,000	
	Federal Share (STBGP or HURF Exchange):	\$40,000	
	Local Match:	\$32,000	
NOTE: HURF Exchange provides 90% of costs up front. The remaining 10% will be reimbursed upon project completion.			
<input checked="" type="checkbox"/> CONSTRUCTION	FY Program Year:	2026	
	Funding Source Request:	<input type="checkbox"/> STBGP	<input checked="" type="checkbox"/> HURF Exchange
	Other Non-Local Funding Sources to be Utilized:	<input type="checkbox"/> _____	
	Total Cost Estimate:	\$391,000	
	Federal Share (STBGP or HURF Exchange):	\$351,900	
	Local Match (10% exchange):	\$39,100	
NOTE: HURF Exchange provides 90% of costs up front. The remaining 10% will be reimbursed upon project completion.			
<ul style="list-style-type: none"> If Utilizing STBGP dollars, ATTACH a completed "ADOT Cost Estimate Tool" document for your estimate. If Utilizing HURF Exchange dollars, ATTACH a completed "HURF Exchange Scoping, Cost and Schedule" document for your estimate. In the event that the proposed project within this application is considered ineligible or is not awarded with HURF Exchange dollars and would still like to be considered for STBGP funds, please fill out the "ADOT Cost Estimate Tool" document in addition to the "HURF Exchange Scoping, Cost and Schedule" document. <p style="text-align: center; color: red; font-weight: bold;">Any application without the required attachment(s) will not be considered for funding.</p>			

PROJECT NEED

This section should clearly state why this project is one of the highest priorities within the CAG Region for which the use of the requested regional funds is the best option (*No more than one page long; Cambria size 10 font*).

PROJECT NEED:

This project was included in the FY2022 program associated with the Golden Hills Project. When the project was developed the cost escalation issue made the funding insufficient, and the detached sidewalk on Main Street was abandoned by ADOT; an AC shoulder (intended to be used as a pathway) was built adjacent to the travel lane. The shoulder width varies from 3 to 4 feet and does not provide a safe passage for pedestrians as it acts primarily as a roadway shoulder. The standard for sidewalks along higher volume roadways require a curb and gutter or be detached from the travel way. To provide pedestrians with a walkway in accordance with standards, the City of Globe proposes a detached sidewalk to save costs. It is anticipated that this pathway will be a 5' concrete surface on aggregate base material. The following is from FHWA guidance for sidewalks: ***"The use of shoulders as a substitute for sidewalks is never justified in urban areas. Sidewalks should be delineated by a vertical and horizontal separation from moving traffic to provide an adequate buffer space and a sense of safety for pedestrians."***



View of Widened Shoulder, not a walkway....



View of built widened Shoulder.....



Sidewalks provide people with space to travel within the public right-of-way that is separated from roadway vehicles.

Desired pathway, separated from the travel lanes.

PROJECT WORK DESCRIPTION

Provide a brief work description that describes the work to be performed, existing and/or proposed conditions, its benefits and overall cost estimate. *(No more than one page long; Cambria size 10 minimum font).* Please **ATTACH a Project Vicinity/Project Location Map on a separate page as part of the overall application.**

PROJECT NEED:

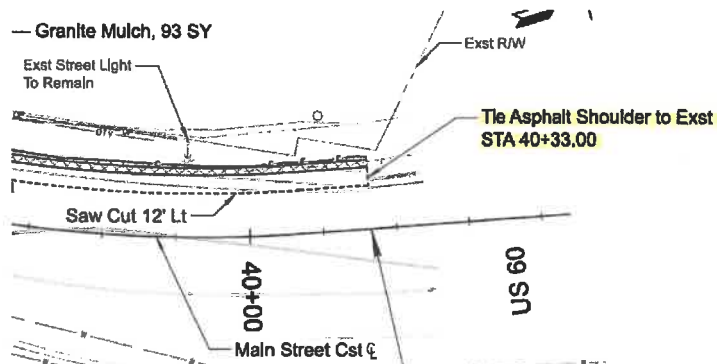
The recently completed project essentially just provides an asphalt shoulder on the road. The shoulder is narrow, and pedestrians are hesitant to utilize this pathway due to the proximity to live traffic. In addition, the city has received numerous complaints about this since the project was completed. One of the main goals of the city council is public safety, this project has been identified as a high need based on council safety priorities. ***The scope of this proposed project is to construct 1990 LF of 5' concrete sidewalk detached from the travel lanes, constructed within the existing right-of-way. The city is requesting construction funds (HURF swap) for this project and will pay for a portion of the design of the project.***

The exhibits below show the previous federal project that just added a shoulder. The plans drawing below reference tying the shoulder into existing road, there is no reference to this being a pedestrian walkway.



MAIN STREET; US60 - GOLDEN HILL RD

PROJECT NO. 0000 GI GGI SL692 02C
FEDERAL AID NO. GGI-0(207)T



ITEMS TO BE ADDRESSED

PROJECT INCLUSION IN PREVIOUS PLANS	Is the project included in previous plans?		<input checked="" type="checkbox"/> YES	<input type="checkbox"/> NO
	<input checked="" type="checkbox"/>	Regional Transportation Plan (RTP)	<input type="checkbox"/>	Pre-Scoping Studies
	<input type="checkbox"/>	Road Safety Assessment (RSA)	<input type="checkbox"/>	Comprehensive Economic Development Strategy (CEDs)
	<input type="checkbox"/>	Capital Improvement Program (CIP)	<input checked="" type="checkbox"/>	Local Comprehensive Plan / General Plan
	<input checked="" type="checkbox"/>	Local Transportation Plan	<input checked="" type="checkbox"/>	Other #1 - Was not constructed properly under previous project _____
	<input type="checkbox"/>	Other #2 _____	<input type="checkbox"/>	Other #3 _____
COMMUNITY TRANSPORTATION BENEFITS	Does the project provide multi-modal improvements? Yes or No and Why?		Yes, it will provide safe passage for pedestrians, and it provides a vital link for pedestrians in the local trailer park access to amenities, since it connects directly with the sidewalk on US 60.	
	Does the project provide Community Investments and/or Economic Development benefits? Yes or No and Why?		Yes, it provides access for citizens who do not own a motor vehicle to vital services, food, banking, and shopping opportunities.	
SAFETY COUNTERMEASURES <i>(For Potential Use of HSIP Funds)</i>	Can you provide crash data, including fatalities over the last five (5) years? Yes or No? <i>(Cite Source of Crash Data)</i>		No.	
	Does the project primarily include any of the 44 safety countermeasures listed on the next page? FHWA safety countermeasures Yes or No?		No.	

SAFETY COUNTERMEASURE		Y or N
1. "Stop Ahead" pavement markings		
2. "Vehicles Entering When Flashing" (VEWF) system (advance post mounted signs on major and loops on minor)		
3. 12-inch signal heads all faces all directions		
4. Actuated advance warning dilemma zone protection system		
5. 3-inch yellow retroreflective sheeting to signal backplates		
6. Advance street name signs		
7. All red clearance interval new or existing signals		
8. All-way stop control (with flashing beacons)		
9. All-way stop control (without flashing beacons)		
10. Composite shoulders (5 feet minimum) on rural two lane roads		
11. 3-lane roadways with center turn lane		
12. Flashing lights and sound signals at Railroad grade crossings		
13. Gates with signs at railroad at grade crossings		
14. Improve 2-lane roadway to 4-lane divided roadway		
15. Improvements that include reducing 11 feet lanes to 9 feet		
16. Install shoulder rumble strips		
17. Install centerline rumble strips		
18. Install wide edgelines (6-inch min)		
19. Install a traffic signal (engineering study demonstrates meeting MUTCD Warrant 7)		
20. Install dynamic signal warning flashers		
21. Install dynamic speed feedback sign at high speed crash curve site with identified speeding problems		
22. Install Intersection Conflict Warning Systems (ICWS) for 4-lane at 2-lane intersections		
23. Install ICWS for 2-lane at 2-lane intersections		
24. Install ICWS with a combination of overhead and advanced post mounted signs (various messages) and flashers		
25. Install ICWS with overhead signs (various messages) and flashers at the intersection on minor; loop on major		
26. Install ICWS with post mounted signs (various messages) and flashers in advance of the intersection on major; loop on major		
27. Modern roundabout where a signalized intersection exists		
28. Roundabout at a high-speed 3 or 4 leg rural intersection		
29. Modify zero or negative left-turn lane offset to create positive offset		
30. New left-turn lanes with positive offset		
31. Pavement friction (Microsurfacing, Open Graded Friction Course, High Friction Surfacing)		
32. Pedestrian Hybrid Beacon (PHB or HAWK)		
33. Position offset left-turn lanes on both major road approaches		
34. Protected only left-turn signal equipment		
35. Protected-permissive left-turn signal equipment		
36. Raised median		
37. Right-turn lane geometry with increased line of sight		
38. Rural 2-lane roads with TWLTL (Two-Way Left Turn Lanes)		
39. Urban 2-lane road with TWLTL		
40. Safety edge treatment on rural highways		
41. Single- or multi-lane roundabout at a 2-way stop-controlled intersection		
42. Single- or multi-lane roundabout at existing signalized intersection		
43. 2-way stop control at uncontrolled neighborhood intersections		
44. Wet-reflective pavement markings		

OTHER CONSIDERATIONS

(Provide Any Supplemental Supporting Documentation – Optional)

ENVIRONMENTAL	<p>Are there any potential environmental impacts or challenges of the project that you can foresee?</p> <p>Yes or No and Why?</p> <p><i>(e.g. endanger species, cultural assets, hazardous materials sites, 4Fs, Title VI populations, wet lands that would be affected, etc.)</i></p>	No, the project will be constructed completely within the city of Globe right of way. There are no major washes or drainages that will be affected by the project.
RIGHT-OF-WAY (ROW)	<p>Please describe any ROW items associated with this project.</p> <p><i>(e.g. Will ROW be required? How much ROW? Is the State Land Department involved?)</i></p>	No new right of way will be required for the project.
DEVELOPMENT ACTIVITY	<p>Is there any planned or ongoing development activity that could impact the proposed project? If Yes, please explain.</p>	No planned development activity.
UTILITIES	<p>Will the project include/require any utility relocation(s) by the project sponsor? If Yes, please explain.</p>	No, the project will be placed along the surface and utilities will not be affected.
DRAINAGE	<p>Are there any drainage issues and/or proposed improvements associated with this project?</p>	No major drainage issues are anticipated. Minor drainage considerations will be addressed during design.

LEVEL OF SERVICE (LOS):

Current:

A

After:

A

Level of Service "A" =	Free-flow traffic with individual users virtually unaffected by the presence of others in the traffic stream.
Level of Service "B" =	Stable traffic flow with a high degree of freedom to select speed and operating conditions but with some influence from users.
Level of Service "C" =	Restricted flow that remains stable but with significant interactions with others in the traffic stream. The general level of comfort and convenience declines noticeably at this level.
Level of Service "D" =	High-density flow in which speed and freedom to maneuver are severely restricted and comfort and convenience have declined even though flow remains stable.
Level of Service "E" =	Unstable flow at or near capacity levels with poor levels of comfort and convenience.
Level of Service "F" =	Forced traffic flow in which the amount of traffic approaching a point exceeds the amount that can be served. LOS F is characterized by stop-and-go waves, poor travel times, low comfort and convenience, and increased accident exposure.

HURF Exchange Project Scoping Form

Note: The Project Scoping Form, Project Cost Estimate and Project and Draw Schedule forms (all contained in this file) must be completed and submitted with the project initiation request. Each phase requires advanced authorization by ADOT. HURF Exchange funding may not be combined with federal aid.

INSTRUCTIONS: This form is required to accompany a HURF Exchange Project Initiation request.

Enter information into GREEN CELLS

PROJECT INFORMATION

(information provided in this section will auto-populate to the Project Cost Estimate and Draw Schedule tabs)

Project Sponsor	City of Globe
Sponsor Contact	Luis Chavez
Contact Phone #	928-961-1748
Contact Email	lchavez@globeaz.gov
Project Name	City of Globe Main Street Pathway
Project Location	Main Street
Functional Classification (select from	Minor Collector
Termini Begin/End	(US 60 - 4th Ave)
ADOT District (select from list)	

ADOT USE ONLY

ADOT Project Number (5 digit)		IGA Number	
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Funding Information

COG/MPO (select from list)	CAAG			
TIP Information	Description	Design	Right of Way	Construction
	Year Programmed	FY 24	N/A	FY 25
	TIP Number			
	HURF Exchange Amount	\$40,000.00	\$0.00	\$351,900.00
	Sponsor Amount	\$32,000.00	\$0.00	\$39,100.00
	Total Amount	\$72,000.00	\$0.00	\$391,000.00

Scope of Work Summary

Delivery Method (select from list)	Advertise for bids
------------------------------------	--------------------

Scoping document attached? (select from list)	Yes
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Major Items of Work (press Alt-Tab to create a new line; press Alt-Tab-Tab to create a new paragraph)	Mobilization, Clearing and grade prep, place AB, set forms and place concrete, apply curing compound, construct ADA ramps, clean up, demobilization, close-out
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HURF Exchange Project Cost Estimate

Note: The Project Scoping Form, Project Cost Estimate and Project and Draw Schedule forms (all contained in this file) must be completed and submitted with the project initiation request. Each phase requires advanced authorization by ADOT. HURF Exchange funding may not be combined with federal aid.

INSTRUCTIONS: This form is required to accompany a HURF Exchange Project Initiation request. List all items necessary to develop and construct the project. The sponsoring agency is responsible for verifying all costs and their accuracy. Construction cost overruns will be the responsibility of the sponsoring agency.

**Enter values
into GREEN
CELLS**

PROJECT INFORMATION

(fields below will be populated based on information entered on the Project Scoping Form tab)

Project Sponsor	City of Globe
Sponsor Contact	Luis Chavez
Contact Phone #	928-961-1748
Contact Email	lchavez@globeaz.gov
Project Name	City of Globe Main Street Pathway
Project Location	Main Street
Termini Begin/End	(US 60 - 4th Ave)
COG/MPO	CAAG
Design TIP Number	0
Right of Way TIP Number	0
Construction TIP Number	0

ADOT USE ONLY

ADOT Project Number

0

STAGE I – SCOPING (15% Preliminary Design)

NOT ELIGIBLE FOR HURF EXCHANGE FUNDING

STAGES II, III, IV and V - DESIGN

DESIGN COSTS

ITEM DESCRIPTION	UNIT	QUANTITY	UNIT PRICE	TOTAL
PS&E's - Plans, Special Provisions, Cost Estimates & Schedules (10%-20% of construction cost.)	Lump Sum	1	\$72,000.00	\$72,000.00
GEOTECHNICAL INVESTIGATION (If a report is necessary, anticipate 5% of construction cost) Includes testing, Geotech Report, Materials & Pavement Design Report) <i>Enter \$0 in Unit Price column if none required.</i>	Lump Sum	1	\$0.00	\$0.00
DRAINAGE REPORT (If a report is necessary, anticipate 5% of construction cost) <i>Enter \$0 in Unit Price column if none required)</i>	Lump Sum	1		\$0.00
STORM WATER POLLUTION PREVENTION PLAN (Required if there is over 1 acre of total disturbance, 1% of construction cost) <i>Enter \$0 in Unit Price column if none required.</i>	Lump Sum	1		\$0.00
SUBTOTAL – PROJECT DESIGN COSTS				\$72,000.00

RIGHT OF WAY COSTS

ITEM DESCRIPTION	UNIT	QUANTITY	UNIT PRICE	TOTAL
RIGHT OF WAY , Costs for pre-acquisition activities (plans, title reports, appraisals, etc)	Lump Sum	1		\$0.00
RIGHT-OF-WAY ACQUISITION (if necessary)	Lump Sum	1		\$0.00
SUBTOTAL – RIGHT OF WAY COSTS				\$0.00

STAGE V – CONSTRUCTION				
SITE ACQUISITION & HARDSCAPE CONSTRUCTION				
ITEM DESCRIPTION	UNIT	QUANTITY	UNIT PRICE	TOTAL
INSTALLATION OF STORMWATER POLLUTION PREVENTION MEASURES (If over 1 acre of disturbance, 5% of construction costs) <i>Enter \$0 in Unit Price column if area of disturbance is less than one acre.</i>	Lump Sum	1		\$0.00
SITE PREPARATION (Clearing and grubbing, plant salvage)	Lump Sum	1	\$40,000.00	\$40,000.00
DEMOLITION				
Sawcut	Linear Foot			\$0.00
Remove Structures and Obstructions	Lump Sum	1		\$0.00
Remove Fencing	Linear Foot			\$0.00
Remove Structural Concrete	Cubic Yard			\$0.00
Remove Asphaltic Concrete Pavement				\$0.00
Remove Concrete Sidewalks, Slabs				\$0.00
				\$0.00
HAZARDOUS MATERIALS ABATEMENT (If applicable; include heavy metals & asbestos; 5% of construction cost) <i>Enter \$0 in Unit Price column if none required.</i>	Lump Sum	1		\$0.00
UTILITY RELOCATION (If necessary) Only the cost of utilities needing relocation as a direct result of the HURF Exchange project is eligible for HURF Exchange.	Lump Sum	1		\$0.00
RETAINING WALL (Concrete; SF of face above the footing)	Square Footage Facing			\$0.00
EARTHWORK				
General Excavation	Cubic Yard			\$0.00
Drainage Excavation				\$0.00
Structural Excavation				\$0.00
Structural Backfill				\$0.00
Borrow (In Place)				\$0.00
CURB & GUTTER	Linear Foot			\$0.00
ROADWAY/PAVING				
Milling	Square Yards			\$0.00
Paving	Tons			\$0.00
AGGREGATE BASE	Cubic Yard	300	\$40.00	\$12,000.00
PATHWAY OR SIDEWALK MATERIALS				
Concrete	Square Foot	10,000	\$30.00	\$300,000.00
Colored Concrete				\$0.00
Stamped Color Concrete				\$0.00
Precast Concrete Pavers				\$0.00
Asphaltic Concrete	Tons			\$0.00
Polymer or Resin Stabilized Surface	Square Foot			\$0.00
CROSSWALK ENHANCEMENT				
Concrete Pavers	Square Foot			\$0.00
Stamped Asphalt				\$0.00
Stamped Concrete				\$0.00
Concrete				\$0.00
Integral Color Concrete				\$0.00
PEDESTRIAN ADA RAMP	Square Foot	100	\$40.00	\$4,000.00
CULVERT EXTENSIONS	Linear Foot			\$0.00
SUBTOTAL - SITE ACQUISITION & HARDSCAPE CONSTRUCTION				\$356,000.00

OTHER CONSTRUCTION ITEMS (List line items)				
ITEM DESCRIPTION	UNIT (Lump Sum, Ton, etc.)	QUANTITY	UNIT PRICE	TOTAL
				\$0.00
				\$0.00
				\$0.00
				\$0.00
				\$0.00
				\$0.00
				\$0.00
				\$0.00
SUBTOTAL - OTHER CONSTRUCTION LINE ITEMS				\$0.00
MOBILIZATION AND ADMINISTRATION COSTS				
ITEM DESCRIPTION	UNIT	QUANTITY	UNIT PRICE	TOTAL
CONTRACTOR MOBILIZATION (Typically 8% of construction cost)	Lump Sum	1	\$25,000.00	\$25,000.00
TRAFFIC CONTROL (0-8% of construction cost)	Lump Sum	1	\$5,000.00	\$5,000.00
CONSTRUCTION SURVEY & LAYOUT (Typically 1% of construction cost)	Lump Sum	1	\$5,000.00	\$5,000.00
CONSTRUCTION CONTINGENCIES (Typically 5% of construction cost)	Lump Sum	1	\$0.00	\$0.00
CONSTRUCTION ADMINISTRATION Typically 20% of construction cost)	Lump Sum	1	\$0.00	\$0.00
SUBTOTAL – MOBILIZATION & ADMINISTRATION COSTS				\$35,000.00
TOTAL STAGE V COSTS (CONSTRUCTION)				\$391,000.00
TOTAL PROJECT COST				\$463,000.00
SUMMARY OF HURF EXCHANGE AND SPONSOR FUNDS				
The data below is automatically calculated based on the information entered above and the amount programmed in the TIP as entered on the Project Scoping Form.				
PHASE	HURF EXCHANGE FUNDS IN TIP	SPONSOR FUNDS	TOTAL	
TOTAL DESIGN	\$40,000.00	\$32,000.00	\$72,000.00	
TOTAL RIGHT OF WAY	\$0.00	\$0.00	\$0.00	
TOTAL CONSTRUCTION	\$351,900.00	\$39,100.00	\$391,000.00	
TOTALS	\$391,900.00	\$71,100.00	\$463,000.00	

Prepared by:

Name Richard Powers
Title PE
Company RPC
Phone 9288121007
Email rlpowers007@gmail.com

HURF Exchange Project and Draw Schedules

Note: The Project Scoping Form, Project Cost Estimate and Project and Draw Schedule forms (all contained in this file) must be completed and submitted with the project initiation request. Each phase requires advanced authorization by ADOT. HURF Exchange funding may not be combined with federal aid.

INSTRUCTIONS: This form is required to accompany a HURF Exchange Project Initiation request. Enter the Estimated Completion Dates as requested for each Stage of Development and Construction. Also enter the Expected Draw Dates for these phases. Draw amounts for each phase are auto-calculated based on the amount programmed in the TIP as entered on the Project Scoping Form.

**Enter dates into
GREEN CELLS**

Project Information

(fields below will be populated based on information entered on the Project Scoping Form tab)

Project Sponsor	City of Globe
Sponsor Contact	Luis Chavez
Contact Phone #	928-961-1748
Contact Email	lchavez@globeaz.gov
Project Name	City of Globe Main Street Pathway
Project Location	Main Street
Termini Begin/End	(US 60 - 4th Ave)
COG/MPO	CAAG
Design TIP Number	0
Right of Way TIP Number	0
Construction TIP Number	0

ADOT USE ONLY

ADOT Project Number

0

Project Development

Development Schedule		HURF Exchange Funding Draw Schedule		
Stage	Estimated Completion Date	Draw %	Amount	Expected Draw Date
Project Initiation	7/24/2024	30%	\$ 12,000.00	N/A
IGA Executed	9/1/2024	30%	\$ 12,000.00	N/A
Request for Authorization to ADOT	9/4/2024	30%	\$ 12,000.00	N/A
Authorization/Start of Work Phase	10/4/2024	10%	\$ 4,000.00	N/A
Stage II	11/20/2024	Total	\$ 40,000.00	
Stage III	12/1/2024			
Stage IV	12/30/2024			
Bid Ready	1/12/2025			

Right of Way

Acquisition Schedule		HURF Exchange Funding Draw Schedule		
Activity	Estimated Completion Date	Draw %	Amount	Expected Draw Date
Request for Authorization to ADOT	N/A	30%	\$ -	N/A
Authorization/Start of Work Phase	N/A	30%	\$ -	N/A
All Parcels Acquired By	N/A	30%	\$ -	N/A
		10%	\$ -	N/A
		Total	\$ -	

Construction

Construction Schedule		HURF Exchange Funding Draw Schedule		
Activity	Estimated Completion Date	Draw %	Amount	Expected Draw Date
Request for Authorization to ADOT	1/15/2025	30%	\$ 105,570.00	6/1/2025
Authorization/Start of Work Phase	2/20/2025	30%	\$ 105,570.00	7/20/2025
Bid Advertisement	3/1/2025	30%	\$ 105,570.00	9/14/2025
Bid Opening	4/1/2025	10%	\$ 35,190.00	10/30/2025
Bid Award	4/15/2025	Total	\$ 351,900.00	
Notice to Proceed to Contractor	5/1/2025			
Substantial Completion	10/15/2025			
Final Project Walk-through	10/16/2025			
Final Acceptance	10/30/2025			

* The Final 10% is reimbursed to the Sponsor as follows:

- ▶ For projects involving multiple phases funded with HURF Exchange - the final 10% for each phase, except for the last, will be reimbursed within 30 days of the receipt and approval of an invoice and documentation demonstrating the phase is complete.
- ▶ At final Project completion - the final 10% will be reimbursed upon completion of the project final voucher by ADOT.

City of Globe Main Street Sidewalk Project (US 60 – 4th Avenue)



Construct:
1990 LF (5' – Detached Sidewalk)

The previous project scope was reduced due to budgetary reasons and the Sidewalk was eliminated, only a 4' shoulder was added, the city requires a 5' concrete detached sidewalk for enhanced safety for pedestrians.



TRANSPORTATION IMPROVEMENT PROGRAM - (TIP)

Last Approved by Regional Council on September 27, 2023

Project #	TRACS #	Sponsor	Project Type	Project Name	From	To	Length (Miles)	Lanes Before	Lanes After	Functional Classification	Federal Aid Type	Federal Funds	HURF Funds Needed	HURF Rate Cost	Local Match	Total Project Funds	Remaining Funds
FY 2019																	
PAY 19-01D	T021101D	PAYSON	DESIGN	GRANITE DELLS RD - (GEOMETRIC CORRECTIONS, PAVEMENT LIFT & MARKINGS, BICYCLE LANES)	HWY 260	MUD SPRINGS RD	0.50	2	2	MAJOR COLLECTOR/ MINOR ARTERIAL	STBGP	\$ -	\$ 180,000.00	\$ 20,000.00	\$ -	\$ 200,000.00	\$ (200,000.00)
FY 2023																	
CAG 23-01P		CAG	N/A	REGIONAL TRAFFIC COUNTING - (FY23-27 CONTRACT)	N/A	N/A	N/A	N/A	N/A	N/A	STBGP	\$ 100,000.00	N/A	N/A	\$ 6,044.54	\$ 106,044.54	\$ (100,000.00)
PAY 21-01C		PAYSON	CONSTRUCTION	GRANITE DELLS RD - (GEOMETRIC CORRECTIONS, PAVEMENT LIFT & MARKINGS, BICYCLE LANES)	HWY 260	MUD SPRINGS RD	0.50	2	2	MAJOR COLLECTOR/ MINOR ARTERIAL	HURF	\$ -	\$ 375,444.00	\$ 41,716.00	\$ -	\$ 417,160.00	\$ (417,160.00)
FY 2024																	
				FY 2024 APPORTIONMENT							STBGP						\$ 506,526.00
				FY 2024 OBLIGATION AUTHORITY AMOUNT - ESTIMATE							STBGP						\$ (32,208.15)
				REPAYMENT IN - (ADOT to CAG) - (From FY23)							STBGP						\$ 714,954.86
				REPAYMENT IN - (ADOT to CAG) - (From FY22)							STBGP						\$ 100,374.70
				TOTAL CREDITS / ADJUSTMENTS - (As of N/A)							STBGP						\$ -
				LOAN OUT - (CAG TO ADOT) - For FY25 Projects & Reprogramming - (NOT YET PROCESSED)							STBGP						\$ (613,713.15)
CAG 24-02P		CAG	N/A	TECHNOLOGY TRANSFER	N/A	N/A	N/A	N/A	N/A	N/A	STBGP	\$ 10,000.00	N/A	N/A	\$ 604.45	\$ 10,604.45	\$ (10,000.00)
PAY 23-01R		PAYSON	ROW	INTERSECTION: W. LONGHORN & S. MCLANE RD - (ROUNDABOUT) - ROW ACQUISITION	N/A	N/A	N/A	N/A	N/A	MINOR ARTERIAL	STBGP	\$ 42,435.00	N/A	N/A	\$ 2,565.00	\$ 45,000.00	\$ (42,435.00)
PAY 26-01D		PAYSON	DESIGN	S. GOODNOW RD	SR 260	E. BONITA ST	0.27	2	2	MINOR ARTERIAL	HURF	\$ -	\$ 150,000.00	\$ 16,666.67	\$ -	\$ 166,666.67	\$ (166,666.67)
SCA 28-01D		SAN CARLOS	DESIGN	BIA 170 - (New Sidewalk) - PENDING APPROVAL - SEP 2023 REGIONAL COUNCIL	N/A	N/A	0.35	1	1	MAJOR COLLECTOR	STBGP	\$ 122,590.00	N/A	N/A	\$ 7,410.00	\$ 130,000.00	\$ (122,590.00)
												\$52,435.00	\$0.00	\$0.00	\$3,169.45	\$55,604.45	\$ 334,242.59
FY 2025																	
				FY 2025 APPORTIONMENT							STBGP						\$ 506,526.00
				FY 2025 OBLIGATION AUTHORITY AMOUNT - ESTIMATE							STBGP						\$ (32,207.15)
				REPAYMENT IN - (ADOT to CAG) - (From FY24) (NOT YET PROCESSED)							STBGP						\$ 613,713.15
				REPAYMENT OUT - (CAG to ADOT) - (GOLDEN HILL ROAD) - (From FY21)							STBGP						\$ (340,244.00)
				REPAYMENT OUT - (CAG to ADOT) - (MAIN STREET) - (From FY21)							STBGP						\$ (137,788.00)
CAG 25-02P		CAG	N/A	TECHNOLOGY TRANSFER	N/A	N/A	N/A	N/A	N/A	N/A	STBGP	\$ 10,000.00	N/A	N/A	\$ 604.45	\$ 10,604.45	\$ (10,000.00)
PAY 28-01C		PAYSON	CONSTRUCTION	S. GOODNOW RD	SR 260	E. BONITA ST	0.27	2	2	MINOR ARTERIAL	HURF	\$ -	\$ 540,000.00	\$ 60,000.00	\$ -	\$ 600,000.00	\$ (600,000.00)
												\$10,000.00	\$0.00	\$0.00	\$604.45	\$10,604.45	\$ -
FY 2026																	
				FY 2026 APPORTIONMENT							STBGP						\$ 506,526.00
				FY 2026 OBLIGATION AUTHORITY AMOUNT - ESTIMATE							STBGP						\$ (32,207.15)
				REPAYMENT OUT - (CAG to ADOT) - (NOT YET PROCESSED) - For FY 2025							STBGP						\$ (159,890.09)
				LOAN IN - (ADOT to CAG) - (NOT YET PROCESSED) - From FY 2027							STBGP						\$ 296,375.09
CAG 26-02P		CAG	N/A	TECHNOLOGY TRANSFER	N/A	N/A	N/A	N/A	N/A	N/A	STBGP	\$ 10,000.00	N/A	N/A	\$ 604.45	\$ 10,604.45	\$ (10,000.00)
												\$10,000.00	\$0.00	\$0.00	\$604.45	\$10,604.45	\$ 600,803.85

Project #	TRACS #	Sponsor	Project Type	Project Name	From	To	Length (Miles)	Lanes Before	Lanes After	Functional Classification	Federal Aid Type	Federal Funds	HURF Funds Needed	HURF Rate Cost	Local Match	Total Project Funds	Remaining Funds
FY 2027																	
				FY 2027 APPORTIONMENT							STBGP						\$ 506,526.00
				FY 2027 OBLIGATION AUTHORITY AMOUNT - ESTIMATE							STBGP						\$ (32,207.15)
				REPAYMENT OUT - (CAG to ADOT) - (NOT YET PROCESSED) - For FY 2026							STBGP						\$ (296,375.09)
				LOAN OUT - (CAG to ADOT) - (NOT YET PROCESSED) - To cover FY 2029 Projects							STBGP						\$ (37,139.91)
CAG 27-02P		CAG	N/A	TECHNOLOGY TRANSFER	N/A	N/A	N/A	N/A	N/A	N/A	STBGP	\$ 10,000.00	N/A	N/A	\$ 604.45	\$ 10,604.45	\$ (10,000.00)
SCA 28-01D		SAN CARLOS	DESIGN	BIA 170 - (New Sidewalk) - PENDING REGIONAL COUNCIL APPROVAL	N/A	N/A	0.35	±	±	MAJOR COLLECTOR	STBGP	\$ 122,590.00	N/A	N/A	\$ 7,410.00	\$ 130,000.00	\$ (122,590.00)
												\$132,590.00	\$0.00	\$0.00	\$8,014.45	\$140,604.45	\$ 130,803.85
FY 2028																	
				FY 2028 APPORTIONMENT							STBGP						\$ 506,526.00
				FY 2028 OBLIGATION AUTHORITY AMOUNT - ESTIMATE							STBGP						\$ (32,207.15)
				LOAN OUT - (CAG to ADOT) - (NOT YET PROCESSED) - To cover FY 2029 Projects							STBGP						\$ (227,959.44)
CAG 29-01P		CAG	N/A	REGIONAL TRAFFIC COUNTING - (FY28-32 Contract) - (Not Yet Executed)	N/A	N/A	N/A	N/A	N/A	N/A	STBGP	\$ 100,000.00	N/A	N/A	\$ 6,044.54	\$ 106,044.54	\$ (100,000.00)
CAG 28-02P		CAG	N/A	TECHNOLOGY TRANSFER	N/A	N/A	N/A	N/A	N/A	N/A	STBGP	\$ 10,000.00	N/A	N/A	\$ 604.45	\$ 10,604.45	\$ (10,000.00)
PAY 28-01D		PAYSON	DESIGN	W. FOREST DR - (MULTI-USE PATH / SIDEWALK)	N. MCLANE RD	SR 87	0.41	2	2	MAJOR COLLECTOR	HURF	\$ -	\$ 95,000.00	\$ 10,555.56	\$ -	\$ 105,555.56	\$ (105,555.56)
												\$10,000.00	\$95,000.00	\$10,555.56	\$604.45	\$116,160.01	\$ 30,803.85
FY 2029																	
				FY 2029 APPORTIONMENT							STBGP						\$ 506,526.00
				FY 2029 OBLIGATION AUTHORITY AMOUNT - ESTIMATE							STBGP						\$ (32,207.15)
				REPAYMENT IN - (ADOT to CAG) - (NOT YET PROCESSED) - From FY 2027							STBGP						\$ 37,139.91
				REPAYMENT IN - (ADOT to CAG) - (NOT YET PROCESSED) - From FY 2028							STBGP						\$ 227,959.44
CAG 29-02P		CAG	N/A	TECHNOLOGY TRANSFER	N/A	N/A	N/A	N/A	N/A	N/A	STBGP	\$ 10,000.00	N/A	N/A	\$ 604.45	\$ 10,604.45	\$ (10,000.00)
PAY 29-01C		PAYSON	CONSTRUCTION	W. FOREST DR - (MULTI-USE PATH / SIDEWALK)	N. MCLANE RD	SR 87	0.41	2	2	MAJOR COLLECTOR	HURF	\$ -	\$ 416,520.00	\$ 46,280.00	\$ -	\$ 462,800.00	\$ (462,800.00)
SCA 30-01C		SAN CARLOS	CONSTRUCTION	BIA 170 - (New Sidewalk)	N/A	N/A	0.35	1	1	MAJOR COLLECTOR	STBGP	\$ 249,404.64	N/A	N/A	\$ 15,075.36	\$ 264,480.00	\$ (249,404.64)
												\$259,404.64	\$416,520.00	\$46,280.00	\$15,679.81	\$737,884.45	\$ 17,213.56
HIGHWAY SAFETY IMPROVEMENT PROJECTS																	
SCA 21-01D	T031301D	SAN CARLOS	DESIGN	WHITE MOUNTAIN RD (BIA 10) & AIRPORT RD - (STREET LIGHT PROJECT)							HSIP - FY21	\$ 300,000.00	N/A	N/A	\$ -	\$ 300,000.00	
SCA 22-01C	T031301C	SAN CARLOS	CONSTRUCTION	WHITE MOUNTAIN RD (BIA 10) & AIRPORT RD - (STREET LIGHT PROJECT)							HSIP - FY24	\$ 678,611.38	N/A	N/A	\$ -	\$ 678,611.38	
GIL 23-02D		GILA COUNTY	DESIGN	HOUSTON MESA ROAD - (PAVED SHOULDERS W/ EL & CL RUMBLE STRIPS)	SR 87	0.4 MILES SOUTH OF NF-198	4.50				HSIP - FY23	\$ 178,227.00	N/A	N/A	\$ 10,773.00	\$ 189,000.00	
GIL 23-03D		GILA COUNTY	DESIGN	CONTROL ROAD - SEGMENT 1 - (PAVED RD/SHOULDERS W/ RUMBLE STRIPS)	SR260	0.35 MILES EAST OF ROBERTS MEAS RD	1.75				HSIP - FY23	\$ 178,227.00	N/A	N/A	\$ 10,773.00	\$ 189,000.00	
GIL 24-01C		GILA COUNTY	CONSTRUCTION	HOUSTON MESA ROAD - (PAVED SHOULDERS W/ EL & CL RUMBLE STRIPS)	SR 87	0.4 MILES SOUTH OF NF-198	4.50				HSIP = FY24	\$ 3,990,651.00	N/A	N/A	\$ 241,216.00	\$ 4,231,867.00	
GIL 24-03C		GILA COUNTY	CONSTRUCTION	CONTROL ROAD - SEGMENT 1 - (PAVED RD/SHOULDERS W/ RUMBLE STRIPS)	SR260	0.35 MILES EAST OF ROBERTS MEAS RD	1.75				HSIP = FY24	\$ 423,571.00	N/A	N/A	\$ 18,722.00	\$ 442,293.00	
												\$5,749,287.38	\$0.00	\$0.00	\$281,484.00	\$6,030,771.38	\$ -
BRIDGE REPLACEMENT PROGRAM																	
GLB 21-01D		GLOBE	DESIGN	PINAL CREEK BRIDGE - COTTONWOOD ST (STRUCTURE #9711) - (FY21)	BROAD ST	COTTONWOOD ST	0.10	2	2	LOCAL	OSB	\$ 330,050.00	N/A	N/A	\$ 19,950.00	\$ 350,000.00	
GLB 22-01C		GLOBE	CONSTRUCTION	PINAL CREEK BRIDGE - COTTONWOOD ST (STRUCTURE #9711) - (FY21)	BROAD ST	COTTONWOOD ST	0.10	2	2	LOCAL	OSB	\$ 669,950.00	N/A	N/A	\$ 480,050.00	\$ 1,150,000.00	
												\$1,000,000.00	\$0.00	\$0.00	\$500,000.00	\$1,500,000.00	\$ -
BUILD GRANTS																	
GIL 22-02C	SS718	GILA COUNTY	CONSTRUCTION	TONTO CREEK BRIDGE & ROADWAY IMPROVEMENTS - (FY22) - BUILD GRANT	SR 188/ OLD HWY 188 INTERSECTION	GREENBACK VALLEY RD - (EAST OF TONTO CREEK)	1.17	1	1	R - MAJOR COLLECTOR	BUILD GRANT	\$ 21,095,564.00	N/A	N/A	\$ 2,825,000.00	\$ 23,920,564.00	
												\$21,095,564.00	\$0.00	\$0.00	\$2,825,000.00	\$23,920,564.00	\$ -

Project #	TRACS #	Sponsor	Project Type	Project Name	From	To	Length (Miles)	Lanes Before	Lanes After	Functional Classification	Federal Aid Type	Federal Funds	HURF Funds Needed	HURF Rate Cost	Local Match	Total Project Funds	Remaining Funds
STATE BUDGET APPROPRIATION FUNDS																	
Funded																	
GLB 22-02C		GLOBE	CONSTRUCTION	HILL ST IMPROVEMENTS - (FY 22)	US 60	"CONNIE'S BRIDGE"	FY 22				STATE	\$ 1,169,400.00	N/A	N/A	\$ -	\$ 1,169,400.00	
GLB 23-01C		GLOBE / GILA COUNTY	CONSTRUCTION	GLOBE/GILA COUNTY SIDEWALK IMPROVEMENTS	MULTI - PHASE	MULTI - PHASE	FY24				STATE	\$ 3,501,100.00	N/A	N/A	\$ 158,000.00	\$ 3,659,100.00	
WKL 23-01C		WINKELMAN / HAYDEN	CONSTRUCTION	WINKELMAN/HAYDEN GOLF COURSE ROAD IMPROVEMENTS	GRIFFIN ST	GOLF COURSE MAINTENANCE RD	FY24				STATE	\$ 1,560,900.00	N/A	N/A	\$ -	\$ 1,560,900.00	
SUP 24-01C		SUPERIOR	CONSTRUCTION	SUPERIOR NEW BRIDGE ON PANTHER DRIVE	SOUTH OF US 60	OVER THE QUEEN CREEK WASH	FY24				STATE	\$ 2,486,700.00	N/A	N/A	\$ 235,799.00	\$ 2,722,499.00	
PAY 24-01C		PAYSON	CONSTRUCTION	INTERSECTION: W. LONGHORN & S. MCLANE RD - (ROUNDABOUT)	N/A	N/A	FY24				STATE	\$ 1,529,800.00	N/A	N/A	\$ 58,405.00	\$ 1,588,205.00	
GLB 24-03C		GLOBE	CONSTRUCTION	HILL ST IMPROVEMENTS - (Additional Funds for Bdrige)	US 60	"CONNIE'S BRIDGE"	FY24				STATE	\$ 643,200.00	N/A	N/A	\$ -	\$ 643,200.00	
GLB 24-04C		GLOBE	CONSTRUCTION	PINAL CREEK BRIDGE - COTTONWOOD ST (STRUCTURE #9711) - (Additional Funds)	BROAD ST	COTTONWOOD ST	FY24				STATE	\$ 632,500.00	N/A	N/A	\$ -	\$ 632,500.00	
GIL 24-04C		GILA COUNTY	CONSTRUCTION	HOUSTON MESA ROAD - (PAVED SHOULDERS W/ EL & CL RUMBLE STRIPS) - (Additional funds)	SR 87	0.4 MILES SOUTH OF NF-198	FY24				STATE	\$ 243,600.00	N/A	N/A	\$ -	\$ 243,600.00	
												\$11,767,200.00	\$0.00	\$0.00	\$452,204.00	\$12,219,404.00	\$ -
CONGRESSIONAL APPROPRIATION FUNDS																	
FY 2023																	
PAY 23-01D		PAYSON	DESIGN	GREEN VALLEY PARKWAY EXTENSION	SR 87	1,250 FT SOUTH OF MAIN STREET	1.00			3/4 R-MAJOR COLLECTOR 1/4 U-MINOR COLLECTOR	CONGRESSIONAL APPROPRIATION	\$ 300,000.00	N/A	N/A	N/A	\$ 300,000.00	
												\$300,000.00	\$0.00	\$0.00	\$0.00	\$300,000.00	\$ -
FY 2024																	
PAY 24-02C		PAYSON	CONSTRUCTION	GREEN VALLEY PARKWAY EXTENSION - <i>(Currently a request & not funded)</i>	SR 87	1,250 FT SOUTH OF MAIN STREET	1.00	0	2	3/4 R-MAJOR COLLECTOR 1/4 U-MINOR COLLECTOR	CONGRESSIONAL APPROPRIATION	\$ 11,336,501.00	N/A	N/A	\$ 685,239.19	\$ 12,021,740.19	
GIL 24-01D		GILA COUNTY	PLANNING/DESIGN	YOUNG ROAD (FS 512) IMPROVEMENTS - <i>(Currently a request & not funded)</i>	COLCORD RD	FS 116	13.50	2	2	R - MINOR COLLECTOR	CONGRESSIONAL APPROPRIATION	\$ 3,300,000.00	N/A	N/A	\$ 199,469.78	\$ 3,499,469.78	
GLB 24-01C		GLOBE	CONSTRUCTION	DOWNTOWN SIDEWALKS IMPROVEMENT PROJECT - <i>(Currently a request & not funded)</i>	NORTH - MESQUITE RD WEST - ASH ST (US 60)	SOUTH - COTTONWOOD ST EAST - HILL ST	VARIES	N/A	N/A	U - MINOR COLLECTOR R - MINOR COLLECTOR	CONGRESSIONAL APPROPRIATION	\$ 3,500,000.00	N/A	N/A	\$ 500,000.00	\$ 4,000,000.00	
GLB 24-02C		GLOBE	CONSTRUCTION	YUMA ST BRIDGE (STRUCTURE #8602) W/SIDEWALK - <i>(Currently a request & not funded)</i>	N BROAD ST	1,500' NE of BROAD ST	0.30	2	2	U - MINOR COLLECTOR	CONGRESSIONAL APPROPRIATION	\$ 3,100,000.00	N/A	N/A	\$ -	\$ 3,100,000.00	
												\$21,236,501.00	\$0.00	\$0.00	\$1,384,708.97	\$22,621,209.97	\$ -
FTA SECTION 5310 GRANTS																	
FY 2024																	
TRAN 24-01		CAG	N/A	CAG/SCMPO MOBILITY MANAGER OPERATIONS - (OCT 1, 2023 - SEP 30, 2024)			5310			MOBILITY MGMT	5310	\$ 110,000.00	N/A	N/A	\$ 27,500.00	\$ 137,500.00	
TRAN 24-02		PAYSON SC	MAINTENANCE	PAYSON SENIOR CENTER - (YR 1 PREVENTATIVE MAINTENANCE)			5310			PREVENATIVE MAINTENANCE	5310	\$ 8,000.00	N/A	N/A	\$ 2,000.00	\$ 10,000.00	
TRAN 24-03		PAYSON SC	SOFTWARE	PAYSON SENIOR CENTER - (YR 1 SCHEDULING SOFTWARE)			5310			SOFTWARE	5310	\$ 20,000.00	N/A	N/A	\$ 5,000.00	\$ 25,000.00	
TRAN 24-04		PAYSON SC	OPERATIONS	PAYSON SENIOR CENTER - (YR 1 OPERATIONS)			5310			OPERATIONS	5310	\$ 35,000.00	N/A	N/A	\$ 35,000.00	\$ 70,000.00	
TRAN 24-05		PAYSON SC	VEHICLE	PAYSON SENIOR CENTER - (REPLACEMENT - ADA FRIENDLY VEHICLE #1)			5310			VEHICLE	5310	\$ 71,666.00	N/A	N/A	\$ 17,916.50	\$ 89,582.50	
TRAN 24-06		PAYSON SC	OPERATIONS	PAYSON SENIOR CENTER - (REPLACEMENT - ADA FRIENDLY VEHICLE #2)			5310			VEHICLE	5310	\$ 105,774.00	N/A	N/A	\$ 26,443.50	\$ 132,217.50	
												\$350,440.00	\$0.00	\$0.00	\$113,860.00	\$464,300.00	\$ -

Project #	TRACS #	Sponsor	Project Type	Project Name	From	To	Length (Miles)	Lanes Before	Lanes After	Functional Classification	Federal Aid Type	Federal Funds	HURF Funds Needed	HURF Rate Cost	Local Match	Total Project Funds	Remaining Funds
FTA SECTION 5311 GRANTS																	
FY 2024																	
TRAN 24-07		PAYSON	OPERATIONS	BEELINE BUS - (YR 2 OPERATIONS)			5311			OPERATIONS	5311	\$219,124.00	N/A	N/A	\$158,676.00	\$377,800.00	
TRAN 24-08		PAYSON	MAINTENANCE	BEELINE BUS - (YR 2 PREVENATIVE MAINTENANCE)			5311			PREVENATIVE MAINTENANCE	5311	\$20,800.00	N/A	N/A	\$5,200.00	\$26,000.00	
TRAN 24-09		PAYSON	ADMINISTRATION	BEELINE BUS - (YR 2 ADMINISTRATION)			5311			ADMINISTRATION	5311	\$96,000.00	N/A	N/A	\$24,000.00	\$120,000.00	
TRAN 24-10		SAN CARLOS	ADMINISTRATION	NNEE BICH'ONII TRANSIT - (YR 2 ADMINISTRATION)			5311			ADMINISTRATION	5311	\$160,000.00	N/A	N/A	\$40,000.00	\$200,000.00	
TRAN 24-11		SAN CARLOS	OPERATIONS	NNEE BICH'ONII TRANSIT - (YR 2 OPERATIONS)			5311			OPERATIONS	5311	\$542,429.92	N/A	N/A	\$392,794.08	\$935,224.00	
TRAN 24-12		SAN CARLOS	MAINTENANCE	NNEE BICH'O NII TRANSIT - (YR 2 PREVENTATIVE MAINTENANCE)			5311			PREVENATIVE MAINTENANCE	5311	\$30,000.00	N/A	N/A	\$7,500.00	\$37,500.00	
TRAN 24-13		SAN CARLOS	INTERCITY	NNEE BICH'O NII TRANSIT - (YR 2 INTERCITY)			5311			INTERCITY	5311	\$44,820.08	N/A	N/A	\$32,455.92	\$77,276.00	
TRAN 24-14		MIAMI	OPERATIONS	COPPER MOUNTAIN TRANSIT - (YR 2 OPERATIONS)			5311			OPERATIONS	5311	\$175,450.00	N/A	N/A	\$127,050.00	\$302,500.00	
TRAN 24-15		MIAMI	MAINTENANCE	COPPER MOUNTAIN TRANSIT - (YR 2 PREVENTATIVE MAINTENANCE)			5311			PREVENATIVE MAINTENANCE	5311	\$24,000.00	N/A	N/A	\$6,000.00	\$30,000.00	
TRAN 24-16		MIAMI	ADMINISTRATION	COPPER MOUNTAIN TRANSIT - (YR 2 ADMINISTRATION)			5311			ADMINISTRATION	5311	\$108,000.00	N/A	N/A	\$27,000.00	\$135,000.00	
												\$1,420,624.00	\$0.00	\$0.00	\$820,676.00	\$2,241,300.00	\$-
OTHER FTA GRANTS																	
TRAN 21-23		PAYSON SC	BUS STOP IMPROV.	BUS ASSOCIATED TRANSIT IMPROVEMENTS / BUS SHELTERS			5339			BUS STOPS	5339	\$610,000.00	N/A	N/A	\$67,777.78	\$677,777.78	